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REPORT
OF THE
TRANSIT DEPARTMENT
FOR THE
YEAR ENDING DECEMBER 31, 1927



CITY OF BOSTON
PRINTING DEPARTMENT
1928

COMPLIMENTS OF

TRANSIT DEPARTMENT—CITY OF BOSTON


THOMAS F. SULLIVAN, *Chairman*,
NATHAN A. HELLER,
JAMES B. NOYES,
Commissioners.



REPORT
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ANNUAL REPORT
OF THE
TRANSIT DEPARTMENT

FOR YEAR ENDING DECEMBER 31, 1927.

1 BEACON STREET, BOSTON, MASS., January 1, 1928.

*To the Mayor and City Council
of the City of Boston:*

The Transit Department submits the following report for the year ending December 31, 1927:

DORCHESTER RAPID TRANSIT.

During 1927 the principal construction work between Andrew Station and Fields Corner Station was completed.

This work has involved the building and equipment of stations, the construction of underpasses and overpasses across the rapid transit line for freight sidings of the New York, New Haven & Hartford Railroad main line, the rebuilding, relocation or widening of highway bridges, the abolition of six grade crossings, and the installation of tracks, signals and power equipment.

In connection with the work proposals were advertised, bids received and contracts let for forty-six different jobs.

Forty-six parcels of real estate were taken by eminent domain, and settlements have been effected with thirty-nine different owners.

In carrying on the work it was necessary to discontinue steam railroad service at Crescent avenue and Savin Hill

stations. By authority of the Department of Public Utilities, and after public hearings, all service was stopped at Crescent avenue on July 15. The outbound Savin Hill service was discontinued on November 1, but the inbound Savin Hill service was continued until November 4, the day before the beginning of the rapid transit service by the Boston Elevated Railway.

The section between Fields Corner and Ashmont is now more than fifty per cent. completed, and it is expected that rapid transit operation can begin to this point sometime in September, 1928.

On November 5, the operation of the rapid transit line between Fields Corner, Dorchester, and Harvard square, Cambridge began. The distance is seven and three-quarters miles, running by way of Dorchester Rapid Transit to a point near Andrew square, thence by Dorchester Tunnel to Park Street Under, then by the Cambridge Connection (Beacon Hill Tunnel) to the West End, thence over the Longfellow Bridge by way of the Cambridge Main Street Subway to Harvard square. The scheduled time is twenty-two minutes, which includes stops at nine intermediate stations. From Fields Corner to Park street the distance is over four and one-half miles, the running time being fourteen minutes. As this line has no branch lines and passes over no drawbridges, it operates closely to schedule at all times.

Central platforms are used at the Columbia road and Savin Hill stations, while side platforms are provided at Fields Corner. The platforms are 300 feet long, but are so designed as to permit extension to 435 feet.

The opening of this line has relieved congestion at Andrew Station, has furnished a conveniently located service for the Savin Hill district, and an adequate and safe transfer point at Fields Corner. During the summer season the Columbia Station gives convenient access to Columbus park and the adjoining beaches.

On January 26, 1927, a contract was made with the Boston Elevated Railway Company covering the temporary use of the above-mentioned section of the line.

BOYLSTON STREET SUBWAY.

No action has yet been taken by the Boston Elevated Railway Company under Chapter 341 of the acts of 1925.

On April 27 His Honor the Mayor transmitted to the Governor of the Commonwealth a draft of a bill to amend the foregoing act by providing that if the rental of the Boylston street extension be the sole cause of increase in the cost of service beyond the income of the company, the trustees shall not be obliged to increase the rate of fare, but that the increased cost shall be met as provided in section 11, chapter 159 of the special acts of 1918.

The bill, however, was not accepted by the general court.

TREMONT STREET SUBWAY.

The plans submitted to the Boston Elevated Railway Company for changes in the stairway covering for the main entrance to Scollay square station having been approved, the original structure was removed and a new type of covering substituted. As this entrance and exit could not be closed, most of the work had to be done at night. Work began in October and in December the new covering was in use. The greater visibility given by the new structure has facilitated the movement of street traffic and rendered the crossing of this busy square much safer for pedestrians. The cost of the work was about \$22,600 of which one-half was charged to the subway and tunnel accounts and one-half paid by the city on account of the Cambridge and Court streets widening.

HYDE PARK STREET RAILWAY.

On January 25, 1927, the old car barn property at Readville was leased for a period of five years.

ESCALATORS.

Safety devices have been added to the escalators in the various tunnels and subway. This work was done by the Boston Elevated Railway Company, the cost being charged to the several tunnel accounts.

SINKING FUNDS.

The following is the condition of the debt and of the sinking funds for the various divisions of the work of the department at the date of this report, as stated by the City Treasurer.

SUBWAY (INCLUDING ALTERATIONS).

(Debt, \$4,416,000, outside debt limit.)

Amount of fund January 1, 1927	\$3,014,055 73	
Received:		
Interest on bank deposits January 1, 1927		
to date	\$657 62	
Interest on investments January 1, 1927		
to date	118,457 50	
Appreciation of investments January 1,		
1927 to date	400 00	
Revenue, etc., January 1, 1927	11,263 00	
	<hr/>	130,778 12
		<hr/>
		\$3,144,833 85
Paid:		
Interest on investments purchased Janu-		
ary 1, 1927 to date	\$463 53	
Premium on investments purchased Janu-		
ary 1, 1927 to date	1,326 23	
	<hr/>	1,789 76
		<hr/>
		\$3,143,044 09

CHARLESTOWN BRIDGE, No. 1.

(Debt, \$750,000, inside debt limit.)

Amount of fund January 1, 1927	\$482,864 86	
Received:		
Interest on bank deposits January 1, 1927		
to date	\$550 10	
Interest on investments January 1, 1927		
to date	19,615 00	
Appropriation for debt	4,213 00	
	<hr/>	24,378 10
		<hr/>
		\$507,242 96
Paid:		
Interest on investments purchased Janu-		
ary 1, 1927 to date	\$861 48	
Premium on investments purchased Janu-		
ary 1, 1927 to date	2,678 72	
	<hr/>	3,540 20
		<hr/>
		\$503,702 76

CHARLESTOWN BRIDGE, No. 2.

(Debt, \$665,000, outside debt limit.)

Amount of fund January 1, 1927		\$548,427 37
Received:		
Interest on bank deposits January 1, 1927 to date	\$591 85	
Interest on investments January 1, 1927 to date	23,197 50	
Revenue, etc., January 1, 1927 to date	7,250 32	
Appreciation of investments January 1, 1927 to date	400 00	
	<hr/>	31,439 67
		\$579,867 04
Paid:		
Interest on investments purchased January 1, 1927 to date	\$432 91	
Premium on investments purchased January 1, 1927 to date	278 00	
	<hr/>	710 91
		<u>\$579,156 13</u>

EAST BOSTON TUNNEL.

(Debt, \$3,334,000, outside debt limit.)

Amount of fund January 1, 1927		\$1,755,774 22
Received:		
Interest on bank deposits January 1, 1927 to date	\$475 23	
Interest on investments January 1, 1927 to date	66,472 50	
Revenue, etc., January 1, 1927 to date	5,846 00	
	<hr/>	72,793 73
		\$1,828,567 95
Paid:		
Interest on investments purchased January 1, 1927 to date	\$36 99	
Premium on investments purchased January 1, 1927 to date	1,176 53	
	<hr/>	1,213 52
		<u>\$1,827,354 43</u>

BOSTON TUNNEL AND SUBWAY.

(Debt, \$8,388,700, outside debt limit.)

Amount of fund January 1, 1927		\$3,068,193 25
Received:		
Interest on bank deposits January 1, 1927 to date	\$625 13	
Interest on investments January 1, 1927 to date	124,929 76	
Revenue, etc., January 1, 1927 to date	66,465 00	
	<hr/>	192,019 89
Carried forward		\$3,260,213 14

<i>Brought forward</i>		\$3,260,213 14
Paid:		
Interest on investments purchased January 1, 1927 to date	\$584 59	
Premium on investments purchased January 1, 1927 to date	3,364 00	
	<hr/>	3,948 59
		<u>\$3,256,264 55</u>

RAPID TRANSIT — CAMBRIDGE CONNECTION.

(Debt, \$1,640,000, outside debt limit.)

Amount of fund January 1, 1927		\$327,692 80
Received:		
Interest on bank deposits January 1, 1927 to date	\$340 11	
Interest on investments January 1, 1927 to date	13,413 00	
Revenue, etc., January 1, 1927 to date	15,214 00	
	<hr/>	28,967 11
		\$356,659 91
Paid:		
Interest on investments purchased January 1, 1927 to date	\$144 03	
Premium on investments purchased January 1, 1927 to date	278 00	
	<hr/>	422 03
		<u>\$356,237 88</u>

BOYLSTON STREET SUBWAY.

(Debt, \$5,395,000, outside debt limit.)

Amount of fund January 1, 1927		\$77,995 71
Received:		
Interest on bank deposits January 1, 1927 to date	\$347 13	
Interest on investments January 1, 1927 to date	2,645 00	
Revenue, etc., January 1, 1927 to date	15,895 50	
	<hr/>	18,887 63
		\$96,883 34
Paid:		
Interest on investments purchased January 1, 1927 to date	\$101 46	
Premium on investments purchased January 1, 1927 to date	858 00	
	<hr/>	959 46
		<u>\$95,923 88</u>

EAST BOSTON TUNNEL EXTENSION.

(Debt, \$2,500,000, outside debt limit.)

Amount of fund January 1, 1927		\$202,411 23
Received:		
Interest on bank deposits January 1, 1927		
to date	\$226 77	
Interest on investments January 1, 1927		
to date	8,523 00	
Revenue, etc., January 1, 1927 to date	4,981 07	
		<hr/> 13,730 84
		<hr/> \$216,142 07
Paid:		
Interest on investments purchased January 1, 1927 to date	\$328 00	
Premium on investments purchased January 1, 1927 to date	720 18	
		<hr/> 1,048 18
		<hr/> <u>\$215,093 89</u>

DORCHESTER TUNNEL.

(Debt, \$12,115,000, outside debt limit.)

Amount of fund January 1, 1927		\$367,432 09
Received:		
Interest on bank deposits January 1, 1927		
to date	\$523 99	
Interest on investments January 1, 1927		
to date	14,138 00	
Revenue, etc., January 1, 1927 to date	66,210 36	
		<hr/> 80,872 35
		<hr/> \$448,304 44
Paid:		
Interest on investments purchased January 1, 1927 to date	\$330 00	
Premium on investments purchased January 1, 1927 to date	1,238 47	
		<hr/> 1,568 47
		<hr/> <u>\$446,735 97</u>

DORCHESTER RAPID TRANSIT.

(Debt, \$7,300,000, outside debt limit.)

(No fund.)

ARLINGTON STATION.

(Debt, \$1,235,000, outside debt limit.)

Amount of fund January 1, 1927		\$9,537 60
Received:		
Interest on bank deposits January 1, 1927		
to date	\$50 47	
Interest on investments January 1, 1927		
to date	320 00	
		<hr/> 370 47
		<hr/> <u>\$9,908 07</u>

EAST BOSTON TUNNEL ALTERATIONS.

(Debt, \$3,900,000, outside debt limit.)

Amount of fund January 1, 1927		\$44,314 08
Received:		
Interest on bank deposits January 1, 1927		
to date	\$229 20	
Interest on investments January 1, 1927		
to date	1,370 00	
Revenue, etc., January 1, 1927 to date	10,199 99	
		<hr/> 11,799 19
		<hr/> \$56,113 27
		<hr/>

HYDE PARK STREET RAILWAY.

(Debt, \$322,000, outside debt limit.)

Amount of fund January 1, 1927		\$25,417 47
Received:		
Interest on bank deposits January 1, 1927		
to date	\$249 28	
Interest on investments January 1, 1927		
to date	810 00	
Appropriation for debt January 1, 1927		
to date	8,195 00	
		<hr/> 9,254 28
		<hr/> \$34,671 75
Paid:		
Interest on investments purchased January 1, 1927 to date	\$35 00	
Premium on investments purchased January 1, 1927 to date	90 33	
		<hr/> 125 33
		<hr/> \$34,546 42
		<hr/>

TREMONT STREET SUBWAY ALTERATIONS — ACTS 1924 — CHAPTER 120

(Debt, \$50,000, outside debt limit.)

(No fund.)

EAST BOSTON ALTERATIONS — ACTS 1924 — CHAPTER 120.

(Debt, \$20,000, outside debt limit.)

(No fund.)

RENTAL BILLS RENDERED TO THE BOSTON ELEVATED RAILWAY COMPANY.

The following is a statement of the bills rendered for rental of the various tunnels and subways:

TREMONT STREET SUBWAY.

March 31, 1927:		
Net cost of subway	\$4,135,098 01	
Rental for one quarter		\$46,519 85
Alterations: net cost	242,673 93	
Rental for one quarter		2,730 08
June 30, 1927:		
Net cost of subway	4,135,204 66	
Rental for one quarter		46,521 05
Alterations: net cost	242,673 93	
Rental for one quarter		2,730 08
September 30, 1927:		
Net cost of subway	4,135,357 84	
Rental for one quarter		46,522 78
Alterations: net cost	242,673 93	
Rental for one quarter		2,730 08
December 31, 1927:		
Net cost of subway	4,139,955 11	
Rental for one quarter		46,574 49
Alterations: net cost	242,673 93	
Rental for one quarter		2,730 08
Total		<u>\$197,058 49</u>

WASHINGTON STREET TUNNEL.

March 31, 1927:		
Net cost of tunnel	\$7,943,169 77	
Rental for one quarter		\$89,360 66
June 30, 1927:		
Net cost of tunnel	7,943,369 58	
Rental for one quarter		89,362 91
September 30, 1927:		
Net cost of tunnel	7,943,524 36	
Rental for one quarter		89,364 65
December 31, 1927:		
Net cost of tunnel	7,943,784 43	
Rental for one quarter		89,367 58
Total		<u>\$357,455 80</u>

CAMBRIDGE CONNECTION.

March 31, 1927:		
Net cost of connection	\$1,640,069 21	
Rental for one quarter		\$19,988 34
June 30, 1927:		
Net cost of connection	1,640,157 14	
Rental for one quarter		19,989 41
September 30, 1927:		
Net cost of connection	1,640,545 24	
Rental for one quarter		19,994 14
December 31, 1927:		
Net cost of connection	1,643,116 56	
Rental for one quarter		20,025 48
Total		<u>\$79,997 37</u>

BOYLSTON STREET SUBWAY.

March 31, 1927:					
Net cost of subway	.	.	.	.	\$5,250,283 77
Rental for one quarter	.	.	.	.	\$59,065 69
June 30, 1927:					
Net cost of subway	.	.	.	.	5,250,283 77
Rental for one quarter	.	.	.	.	59,065 69
September 30, 1927:					
Net cost of subway	.	.	.	.	5,252,285 98
Rental for one quarter	.	.	.	.	59,088 22
December 31, 1927:					
Net cost of subway	.	.	.	.	5,250,249 81
Rental for one quarter	.	.	.	.	59,065 31
Total	.	.	.	.	<u>\$236,284 91</u>

EAST BOSTON TUNNEL EXTENSION.

March 31, 1927:					
Net cost of extension	.	.	.	.	\$2,333,252 11
Rental for one quarter	.	.	.	.	\$26,249 08
June 30, 1927:					
Net cost of extension	.	.	.	.	2,334,459 77
Rental for one quarter	.	.	.	.	26,262 79
September 30, 1927:					
Net cost of extension	.	.	.	.	2,334,494 83
Rental for one quarter	.	.	.	.	26,263 07
December 31, 1927:					
Net cost of extension	.	.	.	.	2,334,494 83
Rental for one quarter	.	.	.	.	26,263 07
Total	.	.	.	.	<u>\$105,038 01</u>

DORCHESTER TUNNEL.

March 31, 1927:					
Net cost of tunnel	.	.	.	.	\$10,813,163 86
Rental for one quarter	.	.	.	.	\$121,648 09
June 30, 1927:					
Net cost of tunnel	.	.	.	.	10,816,363 94
Rental for one quarter	.	.	.	.	121,684 09
September 30, 1927:					
Net cost of tunnel	.	.	.	.	10,819,432 44
Rental for one quarter	.	.	.	.	121,718 62
December 31, 1927:					
Net cost of tunnel	.	.	.	.	10,822,982 39
Rental for one quarter	.	.	.	.	121,758 55
Total	.	.	.	.	<u>\$486,809 35</u>

ARLINGTON STATION.

March 31, 1927:					
Net cost of station	.	.	.	.	\$1,217,885 15
Rental for one quarter	.	.	.	.	\$13,701 21
June 30, 1927:					
Net cost of station	.	.	.	.	1,217,869 35
Rental for one quarter	.	.	.	.	13,701 03
September 30, 1927:					
Net cost of station	.	.	.	.	1,217,772 41
Rental for one quarter	.	.	.	.	13,699 94
December 31, 1927:					
Net cost of station	.	.	.	.	1,217,761 82
Rental for one quarter	.	.	.	.	13,699 82
Total	.	.	.	.	<u>\$54,802 00</u>

EAST BOSTON TUNNEL.

March 31, 1927:		
Net cost of tunnel	\$3,390,842 32	
Rental for one quarter		\$38,146 98
June 30, 1927:		
Net cost of tunnel	3,391,018 37	
Rental for one quarter		38,148 96
September 30, 1927:		
Net cost of tunnel	3,391,167 61	
Rental for one quarter		38,150 64
December 31, 1927:		
Net cost of tunnel	3,394,388 24	
Rental for one quarter		38,186 87
Total		<u>\$152,633 45</u>

HYDE PARK STREET RAILWAY.

March 31, 1927:		
Cost of premises	\$225,000 00	
Rental for one quarter		\$2,531 25
June 30, 1927:		
Cost of premises	225,000 00	
Rental for one quarter		2,531 25
September 30, 1927:		
Cost of premises	225,000 00	
Rental for one quarter		2,531 25
December 31, 1927:		
Cost of premises	231,099 45	
Rental for one quarter		2,599 87
Total		<u>\$10,193 62</u>

EAST BOSTON TUNNEL ALTERATIONS.

March 31, 1927:		
Net cost of alterations	\$3,771,124 23	
Rental for one quarter		\$42,425 15
June 30, 1927:		
Net cost of alterations	3,772,640 93	
Rental for one quarter		42,442 21
September 30, 1927:		
Net cost of alterations	3,774,035 54	
Rental for one quarter		42,457 90
December 31, 1927:		
Net cost of alterations	3,773,961 52	
Rental for one quarter		42,457 07
Total		<u>\$169,782 33</u>

DORCHESTER TUNNEL, SECTION K.

December 31, 1927:		
Net cost of section	\$1,298,010 13	
Rental for 57 days		\$9,121 63
Total		<u>\$9,121 63</u>

TOTALS.

Tremont Street Subway	\$197,058 49
Washington Street Tunnel	357,455 80
Cambridge Connection	79,997 37
Boylston Street Subway	236,284 91
Carried forward	<u>\$870,796 57</u>

<i>Brought forward</i>	\$870,796 57
East Boston Tunnel Extension	105,038 01
Dorchester Tunnel	486,809 35
Arlington Station	54,802 00
East Boston Tunnel	152,633 45
Hyde Park Street Railway	10,193 62
East Boston Tunnel Alterations	169,782 33
Dorchester Tunnel, Section K	9,121 63
	<hr/>
	<u>\$1,859,176 96</u>

STATEMENT OF EXPENSES.

The following is a classified statement of the expenses of the department for the year ending December 31, 1927.

EAST BOSTON TUNNEL.

SECTION B.

Construction Expenses:	
Labor	<u>\$403 76</u>

BOSTON TUNNEL AND SUBWAY.

SECTION 5.

Engineering Expenses:	
Stationery — Supplies	\$1 40
Construction Expenses:	
Labor	323 17
	<hr/>
	\$324 57

SECTION 6.

Construction Expenses:	
Escalators	2,423 11

SECTION 9.

Engineering Expenses:	
Skilled Service	\$6 01
Construction Expenses:	
Construction	\$2 30
Labor	241 44
	<hr/>
	243 74
	<hr/>
	249 75
	<hr/>
	<u>\$2,997 43</u>

CAMBRIDGE CONNECTION.

SECTION 2.

Engineering Expenses:	
Stationery — Supplies	\$1 05
Construction Expenses:	
Construction	\$12 21
Escalators	3,034 68
Field Supplies	75
Labor	466 55
Lighting	8 00
Teaming	<hr/>
	3,522 19
	<hr/>
	\$3,523 24
Stairway No. 8:	
Construction	Cr. 1 34
	<hr/>
	<u>\$3,521 90</u>

DORCHESTER TUNNEL.

SECTION B.

Construction Expenses:	
Escalators	\$4,541 77

SECTION D.

Construction Expenses:	
Escalators	6,970 40

SECTION F.

Construction Expenses:	
Escalators	293 16

SECTION G.

Engineering Expenses:	
Skilled Service	\$86 13
Stationery — Supplies	1 20
	<u>\$87 33</u>

Construction Expenses:	
Property Damages — Takings, <i>Cr.</i> 1,000 00	
	<u>Cr. 912 67</u>

SECTION J.

Engineering Expenses:		
Skilled Service	\$11 35	
Stationery — Supplies	2 00	
	<u>\$13 35</u>	
Construction Expenses:		
Construction	\$5 19	
Escalators	1,586 64	
Field Supplies	20	
Labor	64 50	
Tools	13 10	
	<u>1,669 63</u>	
		1,682 98

SECTION K.

Engineering Expenses:		
Skilled Service	\$71 73	
Stationery — Supplies	3 50	
	<u>\$75 23</u>	
Construction Expenses:		
Construction	\$9 55	
Field	5 17	
Interest	31,000 00	
Labor	1,069 82	
Property Repairs	12 46	
	<u>\$32,097 00</u>	
Property Damages — Takings, <i>Cr.</i> 7,538 02		
	<u>24,558 98</u>	
		24,634 21
		<u>\$37,209 85</u>

BOYLSTON STREET SUBWAY.

SECTION L.

Engineering Expenses:	
Stationery — Supplies	\$22 38
<i>Carried forward</i>	<u>\$22 38</u>

Brought forward \$22 38

SECTION 2.

Engineering Expenses:		
Skilled Service	\$41 02	
Stationery — Supplies	2 10	
	<u> </u>	\$43 12
Construction Expenses:		
Autos	\$6 80	
Construction	540 85	
Field Supplies	7 75	
Labor	24 00	
	<u> </u>	579 40
		<u> </u> 622 52

SECTION 5.

Engineering Expenses:		
Stationery — Supplies	\$5 60	
	<u>Cr. 5 60</u>	
Construction Expenses:		
Construction	\$51 81	
	<u>Cr. 112 39</u>	
		Cr. \$60 58
Field Supplies		15 58
Labor	\$1,920 53	
	<u>Cr. 1,920 53</u>	
Lighting	\$16 80	
	<u>Cr. 16 80</u>	
Tools		9 94
		<u> </u> Cr. 35 06
		<u> </u> \$609 84

EAST BOSTON TUNNEL EXTENSION.

SECTION G.

Engineering Expenses:		
Stationery — Supplies		\$4 95
Construction Expenses:		
Construction	\$233 31	
Field Supplies	43 51	
Labor	1,871 04	
	<u> </u>	2,147 86
		<u> </u> \$2,152 81

ARLINGTON STATION.

Main Station:		
Construction Expenses:		
Construction		Cr. \$254 36
Extension:		
Construction Expenses:		
Construction		Cr. 61 28
		<u> </u> Cr. \$315 64

EAST BOSTON TUNNEL ALTERATIONS.

Engineering Expenses:			
Skilled Service	Cr. \$671	68	
Stationery — Supplies		5	30
			<hr/>
			Cr. \$666 38
Construction Expenses:			
Construction	\$85	99	
Field Supplies	Cr. 29	98	
Labor	3,911	17	
Tools	Cr. 360	81	
			<hr/>
			3,606 37
			<hr/>
			<u>\$2,939 99</u>

HYDE PARK STREET RAILWAY.

Engineering Expenses:			
Professional Advice		\$25	00
Construction Expenses:			
Betterments	\$6,099	45	
Paving		262	67
			<hr/>
	\$6,362	12	
Property Damages — Takings, Cr. 4,100	00		
			<hr/>
			2,262 12
			<hr/>
			<u>\$2,287 12</u>

TREMONT STREET SUBWAY ALTERATIONS — ACTS 1924.

Cambridge and Court Streets Widening:			
Scolley Station Changes:			
Engineering Expenses:			
Skilled Service	\$898	73	
Stationery — Supplies		19	27
			<hr/>
			\$918 00
Construction Expenses:			
Advertising	\$8	38	
Alterations	610	73	
Autos		34	
Construction	1,826	86	
Field Supplies	397	42	
Labor	2,286	62	
Tools		16	82
			<hr/>
			5,147 17
			<hr/>
			\$6,065 17
Park Street Station:			
Engineering Expenses:			
Skilled Service	\$714	01	
Stationery — Supplies		6	00
			<hr/>
			720 01
			<hr/>
			<u>\$6,785 18</u>

CAMBRIDGE AND COURT STREETS WIDENING.

SCOLLEY STATION CHANGES.

Bills outstanding		<u>\$163 82</u>
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EAST BOSTON TUNNEL ALTERATIONS — ACTS 1924.

Cambridge and Court Streets Widening:

Scollay Station Changes:

Engineering Expenses:

Skilled Service	\$437 92	
Stationery — Supplies	3 98	
	<hr/>	\$441 90

Construction Expenses:

Advertising	\$6 87	
Alterations	407 15	
Construction	1,281 09	
Field Supplies	246 97	
Labor	1,462 17	
Tools	10 49	
	<hr/>	3,414 74
		<hr/>
		\$3,856 64

DORCHESTER RAPID TRANSIT.

Office:

Clerks	\$6,442 95	
Commissioners and Secretary	17,500 00	
Furniture	230 89	
Interest	79,000 00	
Lighting	446 17	
Office Boy	714 00	
Printing	1,188 00	
Rental	6,645 82	
Repairs	23 35	
Stationery — Supplies	825 22	
Stenographer — Conveyancer	2,907 30	
Telephone — Telegraph	2,264 48	
	<hr/>	\$118,188 18

Engineering Expenses:

Advertising	\$135 75	
Chief Engineer	8,375 00	
Clerks	4,778 63	
Furniture	163 43	
Instruments	135 99	
Lighting	500 98	
Printing	298 75	
Professional Advice	760 00	
Rental	7,312 51	
Repairs	75 45	
Skilled Service	6,655 75	
Stationery — Supplies	4,576 33	
Stenographers	2,420 15	
Telephone — Telegraph	840 20	
	<hr/>	37,028 92

Miscellaneous Expenses:

Autos	\$4,632 80	
Equipment — Inspection	4,025 66	
Labor	109,852 99	
Material	701,669 78	
Service	7,919 30	
Labor	206 49	
Rental — Yard	4,433 33	
Supplies	19 85	
	<hr/>	832,760 20

Acquisition of Shawmut Branch.	Cr. 5,292 62	
	<hr/>	\$982,684 68

Carried forward \$982,684 68

Brought forward \$982,684 68

SECTION 1.

M. F. Gaddis (Contract 790)	\$217,845 81	
Stations:		
Columbia . . .	\$49,101 20	
Savin . . .	76,067 12	
	<hr/>	125,168 32
Field Offices:		
Crescent . . .	\$118 49	
Savin . . .	160 10	
	<hr/>	278 59
Bridges:		
Freeport . . .	\$24 10	
Freeport Under .	1,472 51	
Savin . . .	1,780 93	
	<hr/>	3,277 54
Advertising . . .	\$19 25	
Autos . . .	68 50	
Clerks . . .	1,591 14	
Construction Sup-		
plies . . .	34,048 91	
E q u i p m e n t—		
Material . . .	17,143 53	
Service . . .	254 09	
Field Supplies . .	679 00	
Inspection . . .	2,482 23	
Instruments . . .	64 33	
Labor . . .	16,087 85	
Light and Power .	188 87	
Main Line Track		
Changes . . .	103,775 40	
Professional Advice	1,050 00	
Property Damages		
— Takings . . .	46,465 22	
Skilled Service . .	17,652 78	
Stationery — Sup-		
plies . . .	214 39	
Teaming . . .	270 85	
Telephone — Tele-		
graph . . .	3 04	
Tools . . .	115 48	
	<hr/>	242,174 86
		<hr/>
		588,745 12

SECTION 2.

A. G. Tomasello & Sons, Inc.		
(Contract 804)	\$220,790 17	
Stations:		
Fields Corner .	\$122,309 30	
Harrison Sq. Sub-		
Station . . .	115,538 00	
	<hr/>	237,847 30
Field Offices:		
Fields Corner		125 08
Bridges:		
Adams . . .	\$792 40	
Clayton . . .	2,200 91	
Dorchester Ave .	804 47	
Dorchester Ave.		
Under . . .	262 18	
	<hr/>	
Carried forward,	\$4,059 96	\$458,762 55 \$1,571,429 80

<i>Brought forward,</i>	\$4,059 96	\$458,762 55	\$1,571,429 80
Freeman . . .	2,301 94		
Geneva Avenue . .	549 76		
		6,911 66	
Advertising . . .	\$19 25		
Autos . . .	782 25		
Borings . . .	439 00		
Clerks . . .	866 00		
Construction:			
(Includes steel-			
material \$22.29)	34,773 06		
E q u i p m e n t—			
Material . . .	2,318 04		
Service . . .	79 67		
Field Supplies . .	1,158 67		
Fuel . . .	681 82		
Inspection . . .	149 04		
Instruments . . .	75 33		
Labor . . .	20,682 34		
Lighting . . .	728 79		
Professional Ad-			
vice . . .	4,900 00		
Property Damages			
— Takings . . .	204,004 53		
Property Repairs . .	4 69		
Skilled Service . .	16,587 61		
Stationery — Sup-			
plies . . .	440 09		
Stenographers . .	276 24		
Teaming . . .	270 87		
Telephone — Tele-			
graph . . .	20 53		
Tools . . .	718 42		
Water Pipes . . .	727 33		
		290,703 57	
			756,377 78

SECTION 3.

C. & R. Construction Co. (Con-			
tract 805)	\$506,113 72		
Station:			
Shawmut	41,060 61		
Field Office:			
Shawmut	235 56		
Bridges:			
Centre	\$4 45		
Mather	1 11		
Melville	1 67		
Park	20 25		
Welles	1 11		
		28 59	
Advertising . . .	\$7 25		
Autos . . .	858 71		
Clerks . . .	1,133 13		
Construction . . .	5,629 41		
E q u i p m e n t—			
Material . . .	5,232 62		
Service . . .	205 27		
Field Supplies . .	990 20		
Inspection . . .	92 33		
Instruments . . .	196 48		

Carried forward, \$14,345 40 \$547,438 48 \$2,327,807 58

<i>Brought forward,</i>	\$14,345 40	\$547,438 48	\$2,327,807 58
Labor . . .	27,992 64		
Professional Ad- vice . . .	86 00		
Property Damages — Takings . .	535 00		
Skilled Service .	23,569 97		
Stationery — Sup- plies . . .	353 97		
Stenographers . .	115 10		
Teaming . . .	1,043 58		
Telephone — Tele- graph . . .	25 53		
Tools . . .	1,113 13		
Water Pipes . .	150 04		
	—————	69,330 36	616,768 84

SECTION 4.

C. & R. Construction Co. (Con- tract 872) . . .	\$156,822 72		
Stations:			
Ashmont . . .	\$78,767 24		
Ashmont Sub. .	4,113 52		
	—————	82,880 76	
Field Office:			
Ashmont . . .		643 05	
Bridges:			
Peabody Square .		350 62	
Advertising . . .	\$379 90		
Autos . . .	566 42		
Borings . . .	84 50		
Clerks . . .	303 68		
Construction . .	53,735 06		
Equipment — Ma- terial . . .	3,605 21		
Field Supplies . .	865 79		
Fuel . . .	278 33		
Inspection . . .	465 27		
Instruments . . .	108 98		
Labor . . .	22,299 09		
Light — Power . .	462 63		
Professional Ad- vice . . .	2,200 00		
Property Damages — Takings . .	171,110 74		
Property Repairs .	78 94		
Skilled Service . .	29,368 17		
Stationery — Sup- plies . . .	626 23		
Teaming . . .	237 39		
Telephone — Tele- graph . . .	7 33		
Tools . . .	1,047 90		
	—————	287,831 56	528,528 71

SECTION 5.

Station:			
Milton . . .	\$158 27		
<i>Carried forward</i> . . .	\$158 27	\$3,473,105 13	

<i>Brought forward,</i>		\$158 27	\$3,473,105 13
Bridges:			
Medway . . .	\$3 36		
Milton . . .	811 21		
		814 57	
Borings . . .	\$559 58		
Instruments . . .	17 50		
Labor . . .	95 81		
Professional Ad- vice . . .	350 00		
Property Damages — Takings . . .	3 44		
Skilled Service . . .	5,674 62		
Stationery — Sup- plies . . .	23 68		
		6,724 63	
			7,697 47
			<u>\$3,480,802 60</u>
Increase:			
East Boston Tunnel		\$403 76	
Boston Tunnel and Subway		2,997 43	
Cambridge Connection		3,521 90	
Dorchester Tunnel		37,209 85	
Boylston Street Subway		609 84	
East Boston Tunnel Extension		2,152 81	
Arlington Station		Cr. 315 64	
East Boston Tunnel Alterations		2,939 99	
Hyde Park Street Railway		2,287 12	
Tremont Street Subway Alterations — Acts 1924		6,785 18	
East Boston Tunnel Alterations — Acts 1924		3,856 64	
Dorchester Rapid Transit		3,480,802 60	
Cambridge and Court Street Widening		163 82	
			<u>\$3,543,415 30</u>

SUMMARY.

	From beginning of work to Dec. 31, 1926.	Jan. 1, 1927, to Dec. 31, 1927.	Total.
Subway — Subway Com- mission	\$14,131 16		\$14,131 16
Part of General Expenses Engineering and miscel- laneous	117,550 71		117,550 71
Section One	407,475 48		407,475 48
Two	239,407 12		239,407 12
Three	363,605 50		363,605 50
Three and one- half	300,639 36		300,639 36
Four	9,355 70		9,355 70
Five	472,147 31		472,147 31
Six	387,411 49		387,411 49
Seven	327,541 86		327,541 86
Eight	231,504 27		231,504 27
Eight and one- half	95,902 06		95,902 06
Nine	76,639 47		76,639 47
Ten	299,452 07		299,452 07
Eleven	254,497 88		254,497 88
	270,310 57		270,310 57
<i>Carried forward . . .</i>	<u>\$3,867,572 01</u>		<u>\$3,867,572 01</u>

	From beginning of work to Dec. 31, 1926.	Jan. 1, 1927, to Dec. 31, 1927.	Total.
<i>Brought forward</i>	\$3,867,572 01		\$3,867,572 01
Interest	258,575 60		258,575 60
	<u>\$4,126,147 61</u>		<u>\$4,126,147 61</u>
Transfer to Alterations, see 11th report	4 95		4 95
	<u>\$4,126,142 66</u>		<u>\$4,126,142 66</u>
Alterations — Part of Gen- eral Expenses	\$28,945 53		\$28,945 53
Section Three	2,568 26		2,568 26
Four	163 42		163 42
Five	30,233 01		30,233 01
Seven	178,516 16		178,516 16
Nine	3 00		3 00
Ten	534 04		534 04
Interest	1,905 56		1,905 56
Transfer from subway, see 11th report.	4 95		4 95
	<u>\$242,873 93</u>		<u>\$242,873 93</u>
Charlestown Bridge: Total	<u>\$1,570,197 98</u>		<u>\$1,570,197 98</u>
Investigation of Conges- tion of Traffic, etc.	<u>\$3,015 92</u>		<u>\$3,015 92</u>
East Boston Tunnel — Part of General Ex- penses	\$161,134 78		\$161,134 78
Engineering and miscel- laneous	199,688 73		199,688 73
Section A	98,869 09		98,869 09
B	1,488,313 23	\$403 76	1,488,716 99
C	508,202 77		508,202 77
D	246,569 26		246,569 26
E	188,307 72		188,307 72
F	243,763 23		243,763 23
Interest	248,156 88		248,156 88
	<u>\$3,383,005 69</u>	<u>\$403 76</u>	<u>\$3,383,409 45</u>
Boston Tunnel and Sub- way — Part of Gen- eral Expenses	\$226,547 21		\$226,547 21
Engineering and miscel- laneous	419,490 59		419,490 59
Section One	815,591 24		815,591 24
Two	614,158 55		614,158 55
Three	683,842 49		683,842 49
Four	1,205,265 07		1,205,265 07
Five	1,080,101 86	\$324 57	1,080,426 43
Six	349,085 68	2,423 11	351,508 79
Seven	139,723 14		139,723 14
Eight	617,152 07		617,152 07
Nine	678,714 43	249 75	678,964 18
<i>Carried forward</i>	\$6,829,672 33	\$2,997 43	\$6,832,669 76

	From beginning of work to Dec. 31, 1926.	Jan. 1, 1927, to Dec. 31, 1927.	Total.
<i>Brought forward</i> . . .	\$6,829,672 33	\$2,997 43	\$6,832,669 76
Ten . . .	142,835 42		142,835 42
Eleven . . .	345,493 91		345,493 91
Twelve . . .	45,417 52		45,417 52
Interest . . .	648,179 81		648,179 81
	<u>\$8,011,598 99</u>	<u>\$2,997 43</u>	<u>\$8,014,596 42</u>
Cambridge Connection — Part of General Ex- penses . . .	\$67,261 25		\$67,261 25
Engineering and miscel- laneous . . .	258,505 56		258,505 56
Section One . . .	590,277 28		590,277 28
Two . . .	640,111 45	\$3,521 90	643,633 35
Interest . . .	76,722 00		76,722 00
	<u>\$1,632,877 54</u>	<u>\$3,521 90</u>	<u>\$1,636,399 44</u>
Dorchester Tunnel — Part of General Expenses .	\$197,392 47		\$197,392 47
Engineering and miscel- laneous . . .	833,272 33		833,272 33
Section A . . .	409,378 71		409,378 71
B . . .	879,930 67	\$4,541 77	884,472 44
C . . .	460,379 33		460,379 33
D . . .	1,120,397 33	6,970 40	1,127,367 73
E . . .	2,366,598 58		2,366,598 58
F . . .	867,383 95	293 16	867,677 11
G . . .	616,158 18	* 912 67	615,245 51
H . . .	892,879 48		892,879 48
J . . .	962,651 40	1,682 98	964,334 38
K . . .	1,267,375 92	24,634 21	1,292,010 13
Interest . . .	1,312,320 20		1,312,320 20
	<u>\$12,186,118 55</u>	<u>\$37,209 85</u>	<u>\$12,223,328 40</u>
Boylston Street Subway — Part of General Ex- penses . . .	\$104,155 53		\$104,155 53
Engineering and miscel- laneous . . .	240,002 03		240,002 03
Section One . . .	761,581 08	\$22 38	761,603 46
Two . . .	1,232,170 36	622 52	1,232,792 88
Three . . .	585,564 58		585,564 58
Four . . .	1,458,935 20		1,458,935 20
Five . . .	729,165 69	* 35 06	729,130 63
Interest . . .	320,194 59		320,194 59
	<u>\$5,431,769 06</u>	<u>\$609 84</u>	<u>\$5,432,378 90</u>
East Boston Tunnel Ex- tension — Part of General Expenses .	\$38,383 04		\$38,383 04
Engineering and miscel- laneous . . .	976,250 79		976,250 79
<i>Carried forward</i> . . .	\$1,014,633 83		\$1,014,633 83

* Decrease.

	From beginning of work to Dec. 31, 1926.	Jan. 1, 1927, to Dec. 31, 1927.	Total.
<i>Brought forward</i>	\$1,014,633 83		\$1,014,633 83
Section G	326,439 56	\$2,152 81	328,592 37
H	673,456 36		673,456 36
J	135,724 66		135,724 66
Interest	224,138 91		224,138 91
	<u>\$2,374,393 32</u>	<u>\$2,152 81</u>	<u>\$2,376,546 13</u>
Arlington Station — Part of General Expenses	\$41,313 26		\$41,313 26
Engineering and miscel- laneous	72,952 00		72,952 00
Construction	580,268 28	* \$255 36	580,012 92
Extension	483,064 06	* 60 28	483,003 78
Interest	55,738 68		55,738 68
	<u>\$1,233,336 28</u>	<u>* \$315 64</u>	<u>\$1,233,020 64</u>
East Boston Tunnel Al- terations — Part of General Expenses	\$75,407 79		\$75,407 79
Engineering and miscel- laneous	172,434 03	* \$666 38	171,767 65
Construction	3,403,525 52	3,606 37	3,407,131 89
Interest	168,217 76		168,217 76
	<u>\$3,819,585 10</u>	<u>\$2,939 99</u>	<u>\$3,822,525 09</u>
Hyde Park Street Railway — Part of General Ex- penses	\$2,195 04		\$2,195 04
Engineering and miscel- laneous	311,373 86	\$2,287 12	313,660 98
	<u>\$313,568 90</u>	<u>\$2,287 12</u>	<u>\$315,856 02</u>
Tremont Street Subway Alterations — Acts 1924, Part of General Expenses	\$1,093 59		\$1,093 59
Engineering and miscel- laneous	819 20		819 20
Adams Station	2,401 75		2,401 75
Brattle Street — Easter- ly Platform	7,723 72		7,723 72
Hanover Street	28 56		28 56
Haymarket Station	15,161 01		15,161 01
Park Street Station — North Platform	4,486 41		4,486 41
Column Changes		\$720 01	720 01
Scollay Station	420 80		420 80
Scollay Station Changes— Cambridge and Court Streets Widening		6,065 17	6,065 17
	<u>\$32,135 04</u>	<u>\$6,785 18</u>	<u>\$38,920 22</u>

* Decrease.

	From beginning of work to Dec. 31, 1926.	Jan. 1, 1927, to Dec. 31, 1927.	Total.
Cambridge and Court Streets Widening — Bills due from Street Laying-Out Depart- ment		\$163 82	\$163 82
East Boston Tunnel Al- terations, Acts 1924 .			
Atlantic Station . . .	\$2,547 08		\$2,547 08
East Boston Tunnel . .	5,105 13		5,105 13
Scollay Station . . .	48 72		48 72
Scollay Station Changes — Cambridge and Court Streets Widen- ing		\$3,856 64	3,856 64
	<u>\$7,700 93</u>	<u>\$3,856 64</u>	<u>\$11,557 57</u>
Dorchester Rapid Transit — Part of General Expenses	\$91,318 91	\$39,188 18	\$130,507 09
Engineering and miscel- laneous	1,110,489 65	864,496 50	1,974,986 15
Section One	1,119,796 26	588,745 12	1,708,541 38
Two	255,088 01	756,377 78	1,011,465 79
Three	122,320 30	616,768 84	739,089 14
Four	18,223 43	528,528 71	546,752 14
Five	12,900 64	7,697 47	20,598 11
Interest	80,666 67	79,000 00	159,666 67
	<u>\$2,810,803 87</u>	<u>\$3,480,802 60</u>	<u>\$6,291,606 47</u>
Chapter 78 — Resolves of 1913	<u>\$389 14</u>		<u>\$389 14</u>
Chapter 84 — Resolves of 1913	<u>\$636 58</u>		<u>\$636 58</u>
Dorchester Tunnel Exten- sion	<u>\$520 19</u>		<u>\$520 19</u>
Grand Totals	<u>\$47,180,669 67</u>	<u>\$3,543,415 30</u>	<u>\$50,724,084 97</u>

The report of the Chief Engineer giving the work in detail follows.

THOMAS F. SULLIVAN,
NATHAN A. HELLER,
JAMES B. NOYES,
Commissioners.

REPORT OF THE CHIEF ENGINEER.

BOSTON, December 31, 1927.

THOMAS F. SULLIVAN, NATHAN A. HELLER AND JAMES B. NOYES, *Commissioners, City of Boston Transit Department.*

GENTLEMEN,— I herewith submit a report for the year ending December 31, 1927.

Construction and equipment work on several sections of the Dorchester Rapid Transit has progressed rapidly throughout the entire year. About two and one-half miles of the new work, comprising Section K, Dorchester Tunnel, and Sections One and Two, Dorchester Rapid Transit, have been completed and opened for public use during the year. The construction work on Section Three, the covered section, has been completed and this section is now being equipped. Section Four, the Ashmont Section, has been started and is over one half completed. Plans are now being prepared to cover the work on the next section in order that actual construction may be started early in the spring.

The engineering organization has remained practically the same as in the preceding year, there being very little change in the personnel. All field construction work has been under the direction of Assistant Chief Engineer Wilbur W. Davis. The office work has been under the direct supervision of Designing Engineer Leonard B. Howe.

The principal items of new construction work for which plans and specifications have been prepared and contracts let during the year are as follows:

Plumbing, Columbia Station.

Entrance and Exit Building, Stairway and Platform Canopy, Savin Hill Station.

Plumbing, Savin Hill Station.

Sewer in Hubbard Road, Section One, Dorchester Rapid Transit.

Repaving Columbia Road, Section One, Dorchester Rapid Transit.

Painting and Carpenter Work, Fields Corner Station.

Roofing, Fields Corner Station.

Plastering, Fields Corner Station.

Plumbing, Fields Corner Station.

Hand Rails, Fields Corner Station.

Enclosed Area and Heating Plant, Fields Corner.

Wire Fence, Sections One, Two and Three, Dorchester Rapid Transit.
Illuminated Signs, Sections One, Two and Three, Dorchester Rapid Transit.
Entrance and Exit Building, Shawmut Station.
Plumbing, Shawmut Station.
Plastering, Shawmut Station.
Power Conduits and Foot Walk, Sections Three and Four, Dorchester Rapid Transit.
Construction, Section Four, Dorchester Rapid Transit.
Ashmont Sub Power Station, Section Four, Dorchester Rapid Transit.
Removing Scollay Station Entrance and Exit Building, Tremont Street Subway.
Polished Granite Parapet, Scollay Station, Tremont Street Subway.
Contracts for Equipment, Sections Three and Four, Dorchester Rapid Transit.

DORCHESTER RAPID TRANSIT. SECTION ONE.

The construction work on this section, which was in progress at the end of the year, was continued with but little interruption during the early winter months of the new year. The relocation of the New Haven Railroad's four main line tracks was carried on in conjunction with the work of the general contractor for this section, and each of the relocated railroad tracks was put in service as soon as it was lined up and the ballast properly tamped. All of these tracks were in operation by spring and the rapid transit right of way was then cleared to permit the construction of signal and power conduits, preparation of sub-grade, placing of ballast, ties and rails for the new third-rail rapid transit tracks.

In order to do the equipment work near the old Crescent Avenue Railroad Station it was necessary to discontinue the service at this point and close the station which up to this time had been kept open without interrupting the service. This was done on July 15 by authority from the Department of Public Utilities. The station building which was an old wooden structure was torn down shortly afterwards by the Lowell Building Wrecking Company.

Conditions at Savin Hill were entirely different than at Crescent Avenue and made it possible to plan the work at that station in such a manner that steam train service could be continued practically up until the time of starting operation of third-rail trains on the new rapid transit extension. The platform for outbound steam trains at this station was closed on November 1 and the platform for inbound trains on the evening of November 4, the night before the new line was opened.

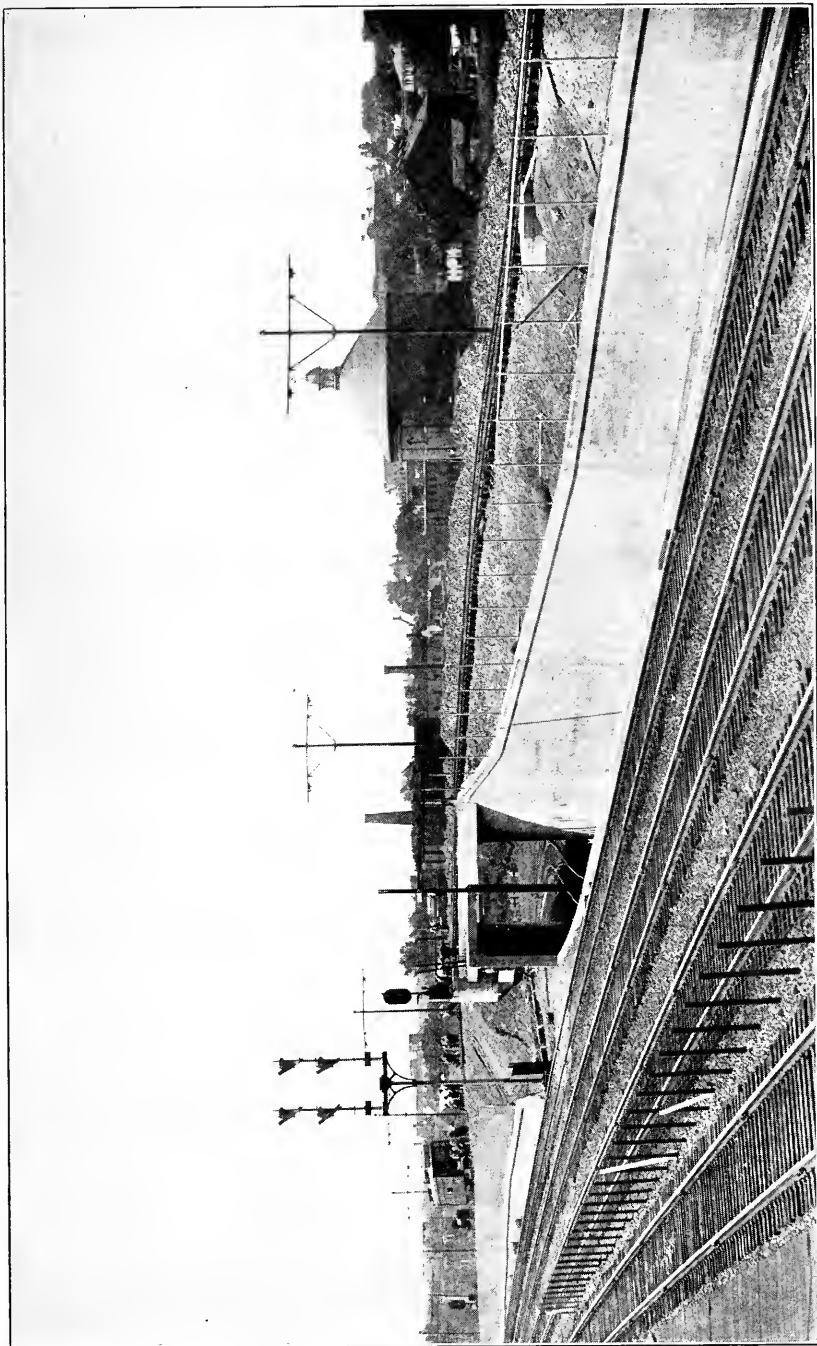


PLATE I.—FREEPORT STREET UNDERPASS.



The temporary railroad station building located on the inbound platform was removed shortly after the steam train service was discontinued.

In the vicinity of Savin Hill Bridge and the Freeport Street underpass several shifts of the railroad tracks had been necessary in order to permit the construction of the bridge, the station and the underpass. The various steps that it was necessary to take as the work proceeded were outlined in the previous report. The work has since been carried through as planned, the work of the general contractor alternating with that of the railroad company.

At the close of last year the work at the Savin Hill Bridge had reached a point where the next step required was the removal of the old east abutment in order to allow the permanent relocation of the two westerly railroad tracks. With the removal of this abutment and the relocation of the tracks it was then possible to build the northerly half of the new westerly abutment. Following this work the new steel and reinforced concrete bridge over the rapid transit right of way was built, and traffic was transferred thereon from the temporary bridge. The removal of the temporary bridge followed immediately and the southerly half of the bridge was then built. The necessary repaving was done at both ends of the bridge and the entire bridge then thrown open to public travel.

The completion of the Freeport Street Underpass followed that of the Savin Hill Bridge and permitted the laying of the permanent freight track through it into the Freeport Street Yard. The temporary freight track leading into the yard was then removed as fast as possible, as it crossed diagonally over the location of the new rapid transit right of way.

Plate I shows the underpass for the freight track and also the construction of the rapid transit tracks which pass over it.

With the relocation of the railroad main line tracks completed, the general contractor for Section One, M. F. Gaddis, started on the third or last period of his contract. The principal items of work in this period were construction of signal and power conduits, signal stands, power manholes, cross conduits, embankments, grading and finishing such other items of construction as it had been possible under the provisions of the contract to carry on throughout the three periods into which the time of the contract was divided. The certificate of completion was given on July 14.

COLUMBIA STATION.

A description of this station will be found in the last annual report at which time the construction was well along. The station was completed early in May of the current year. The plumbing work for the station was done under contract with Edward C. Kelly, the lowest of eight bidders. This station, which was the first one along the line of the new extension beyond Andrew Square, was finished and equipped during the summer in order to permit of operation of this completed portion of the line should it have been decided to operate it.

Plate II shows the entrance and exit building at street level with enclosed stairway to rapid transit island platform below.

HUBBARD ROAD SEWER.

The Savin Hill cut, where carried through Rockdale Street, just south of the Savin Hill Bridge, required the abandoning of the sewer in this location and necessitated rebuilding the old Hubbard Road sewer in order that it might be used to replace the one removed from Rockdale Street. The contract for this work was awarded to A. G. Tomasello & Son, Inc., the lowest of ten bidders. The sewer was built largely through ledge for a length of 366 feet. It was of twelve inch vitrified pipe. Work was started by the contractor on November 15 and completed on December 15.

WIRE FENCE.

The open right of way extends from the portal at Section K (just beyond Andrew Square) to the covered section near Park Street, Dorchester, a distance of two and one-half ($2\frac{1}{2}$) miles.

In order to minimize the danger to persons attempting to cross the right of way over which high speed third rail trains are running on close headway, a high wire fence on each side has been built along the property line. This fence is of the heavy chain link type and is built upon a continuous concrete curb with concrete post foundations spaced about ten feet apart. The wire fence is fabric of two inch mesh No. 6 heavy galvanized welded wire six feet in height. Above the top rail of the fence are three strands of galvanized barbed wire spaced about five inches apart making a total height above curb of seven feet three inches ($7' 3''$). Gates with ten foot openings are provided at Crescent Avenue, Bay Street and at the

City of Boston
Transit
Department



1 Beacon Street
Boston

Received of the Boston Transit Department:

REPORT FOR THE YEAR ENDING DECEMBER 31, 1927

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PLATE II.—COLUMBIA STATION.



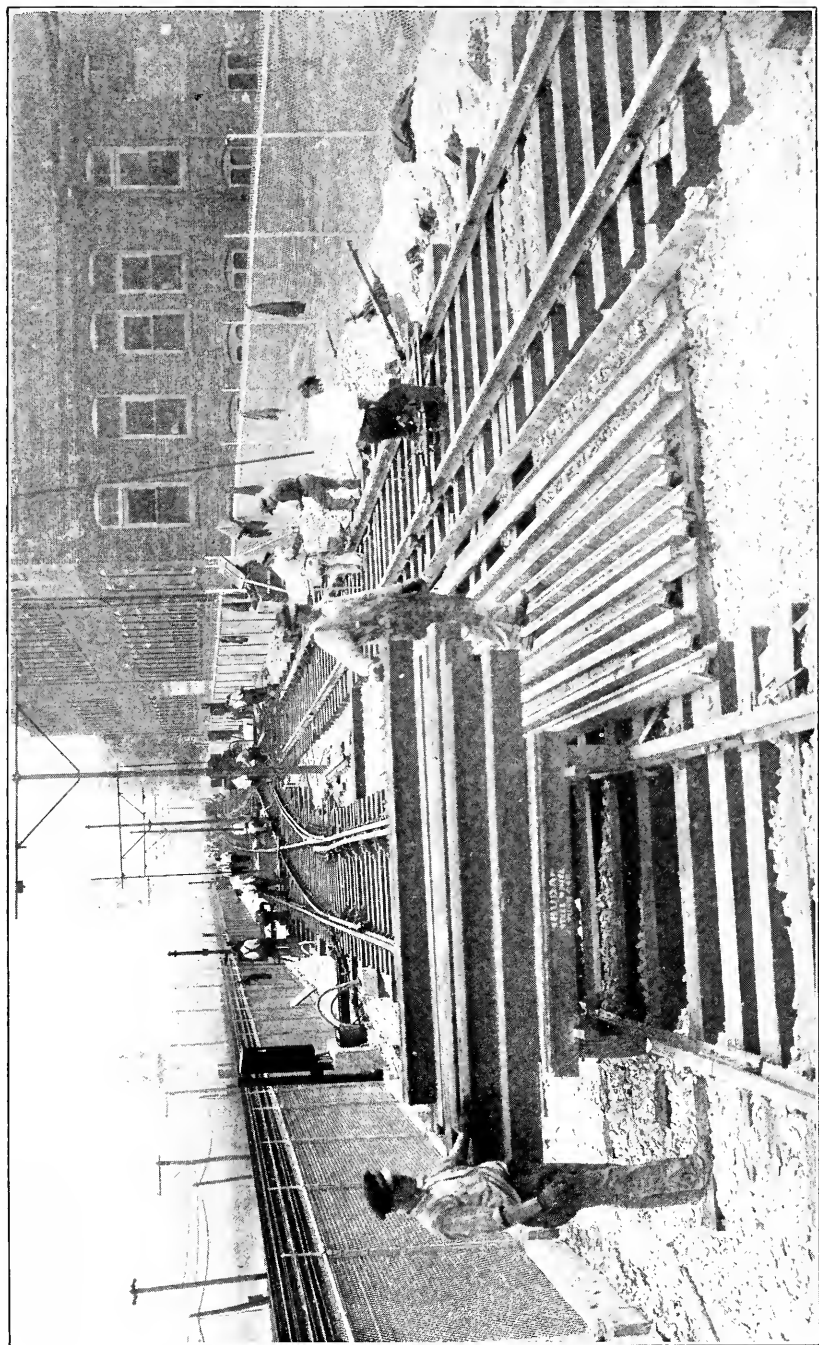


PLATE III.—GENERAL VIEW, OPEN RIGHT OF WAY, SHOWING WIRE FENCE ON EACH SIDE.



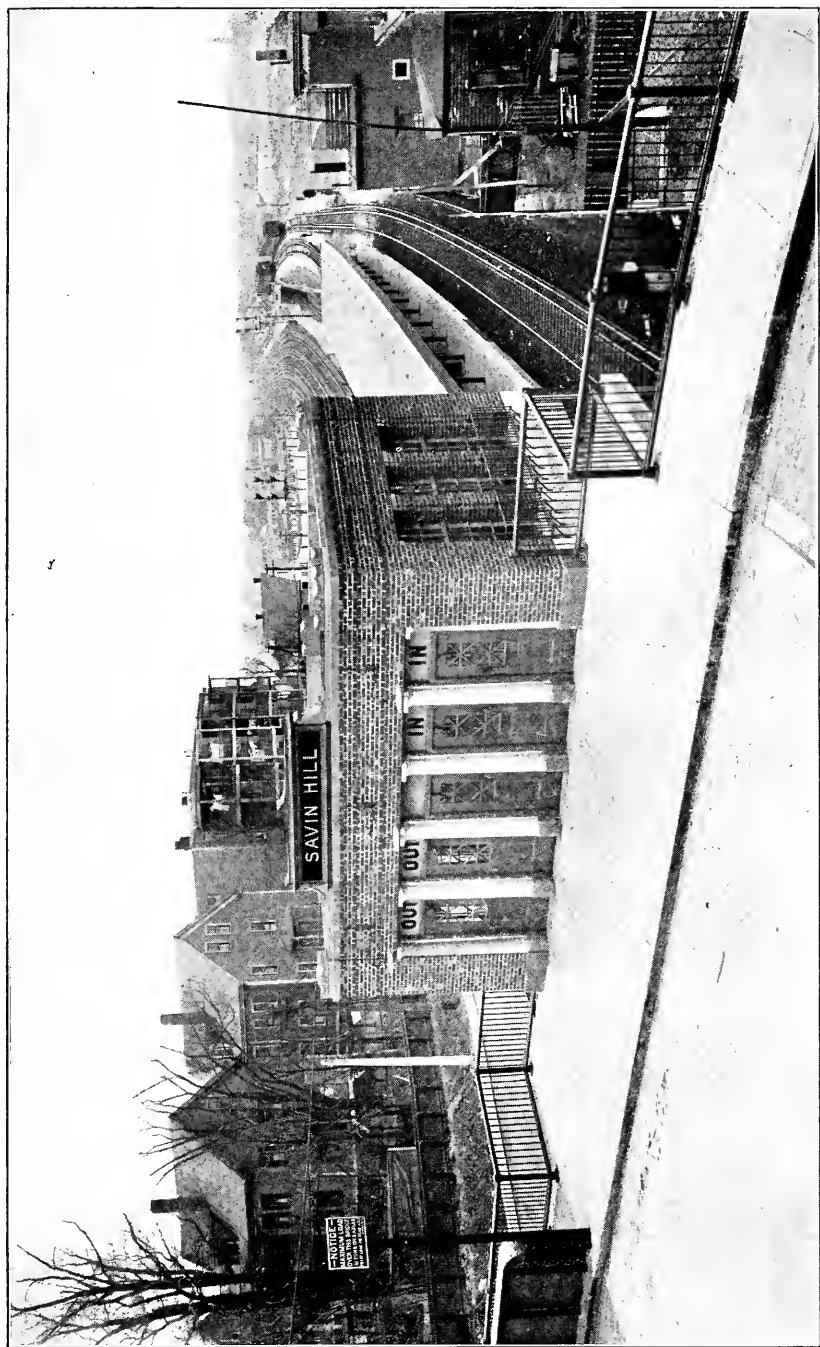


PLATE IV.—SAVIN HILL STATION.



Harrison Square Sub Station for the purpose of allowing track materials, etc., to be brought in.

The entire length of wire fencing and curbing constructed thus far during the year is 24,799 feet, or nearly five miles. The fence work was let out in five sections and built by the following contractors:

American Chain Link Fence Company	One Section
J. A. Singarella	Two Sections
C. A. Gates Company	One Section
A. G. Tomasello & Son, Inc.	One Section

Plate III shows concrete curbing and wire fence in a general view taken along the open right of way while the equipment work was being carried on.

SAVIN HILL STATION.

The actual construction of the station building, stairways and platform canopy was started as soon as the Savin Hill Bridge was completed. This building, as well as the stations at Fields Corner and at Shawmut, was designed by William D. Austin, Consulting Architect. The station is similar in many respects to the Columbia Station, the floor of the head house being at the level of the bridge over the tracks, and having an enclosed stairway leading down to an island platform. The platform is three hundred (300) feet long and is covered its entire length with a fireproof canopy constructed of steel and reinforced concrete. The contract for this work was let to J. A. Singarella, the lowest of nine bidders. The walls of the head house are built of tapestry brick, the floor and roof being of reinforced concrete. The interior is lined with white enameled bricks, the ceilings being of white plaster and the floor of granolithic. The windows are steel casement sashes in steel frames for both the building and the enclosed stairway. There are toilet rooms and switch rooms located beneath the stairway at the platform level. A waiting room with station master's room at one end is located on the platform about 80 feet from the foot of the stairway. The plumbing for the station was done under contract with Edward C. Kelly.

Plate IV shows the station building at the street level, the platform and the outbound tracks below. The rapid transit tracks over the Freeport Street freight track underpass may be seen in the distance.

EQUIPMENT.

Equipment materials that were ordered by the Department under various contracts were delivered early in the year and the actual work of laying tracks was started in April by the Boston Elevated Railway under contract with the Department. The work was carried on continuously thereafter until completed through to Fields Corner the first part of November.

The first delivery of ballast for the trackwork was made near the head of the incline at Section K and the stretch of track from the incline to a point near Columbia Road was the first to be completed. Ballast was delivered by truck at different sections along the line of the work as fast as the subgrading was completed. The ballast was delivered under contracts made by the Department with the Old Colony Crushed Stone Company and with William J. Barry.

Creosoted ties were placed as fast as the ballast was spread. Rails were then brought in on work cars of the Elevated Railway. Temporary trolley poles were erected and trolley wires strung for use of the work cars. These were later removed, just prior to the opening up of the third rail service.

Cable was pulled through the conduits and signal and power installation followed along with the track work. Lighting installation at stations was carried on simultaneously with installation of fare collection equipment, ticket offices, signs, etc.

SECTION TWO.

A. G. Tomasello & Son, Inc., the contractor for construction of this section, continued work throughout the winter months on foundations, retaining walls, bridges and the busway for Fields Corner Station. In the early spring they started work on the superstructure for this station. This station, unlike the two previous stations, is built of structural steel with reinforced concrete walls. Being a transfer station, provision is made for trolley cars and buses as well as third rail trains. The station, including its four trolley car ramps, extends in length about one-quarter mile from Dorchester Avenue, across Freeman Street and as far as Geneva Avenue. Two trolley car ramps on a five percent grade run up to the east end of the station from Dorchester Avenue and two similar ramps approach the west end from Geneva Avenue. A concrete road or busway approaches from the south from the Boston



PLATE V.—BUSWAY AND PLATFORM UNDER FIELDS CORNER STATION.



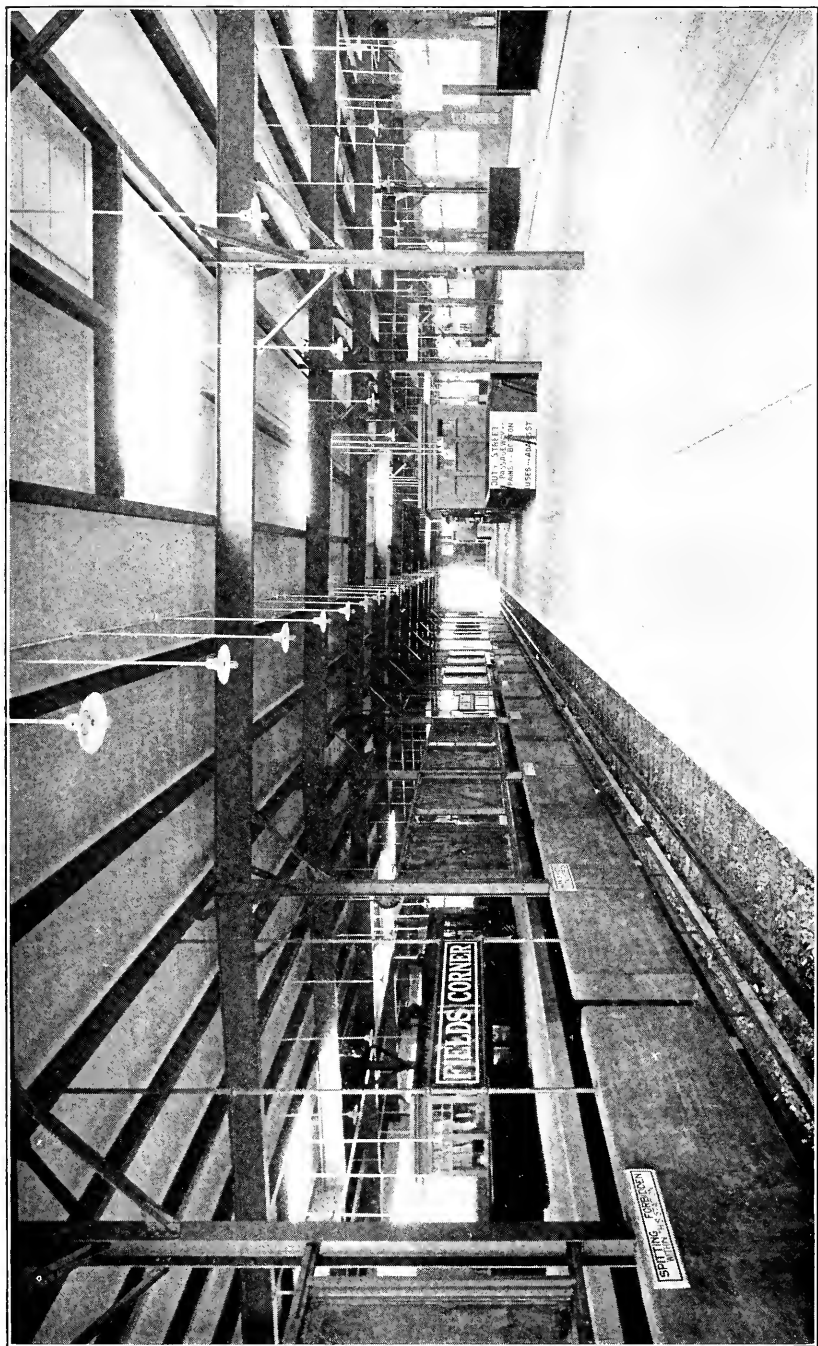


PLATE VI.—TRAIN AND SURFACE CAR PLATFORMS, FIELDS CORNER STATION.



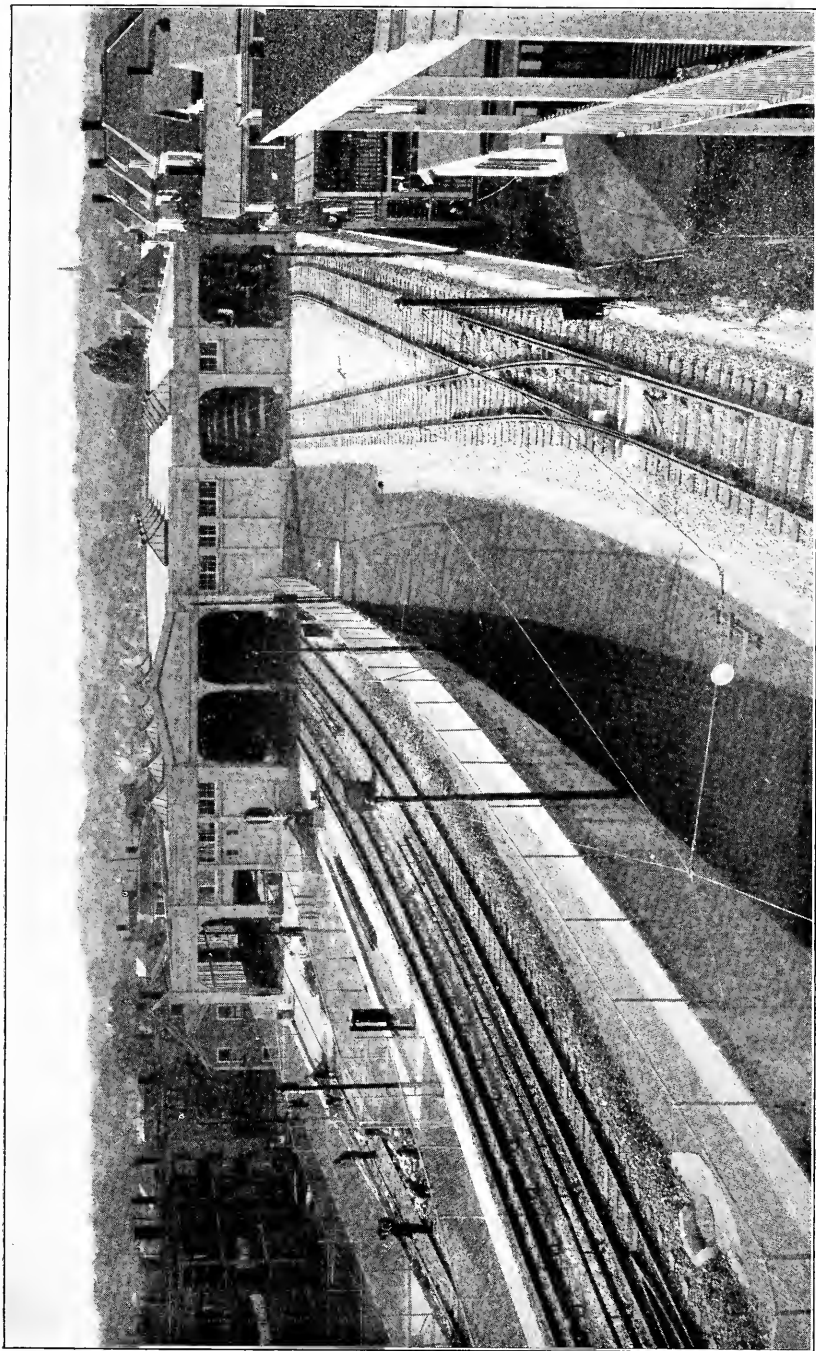


PLATE VII.—GENERAL VIEW FIELDS CORNER STATION, SHOWING THIRD RAIL TRACKS IN CENTER AND
SURFACE CAR TRACKS ON SIDE RAMPS.

Elevated Yard at the corner of Dorchester Avenue and Park Street and then crosses Faulkner Street swinging westerly through the station at the lower level under the inbound surface car tracks. It then leaves the station crossing Freeman Street at grade and running westerly to Geneva Avenue. A fourteen foot platform over 300 feet long is provided at the busway level directly under the station.

Plate V shows busway and platform.

Two passageways with stairways lead from the busway platform to the three platforms on the upper level, where there are also five lines of tracks. The southerly track is for inbound surface cars, the two central tracks are for third rail rapid transit trains and the two northerly tracks are for outbound surface cars. This enclosed upper platform and car track area is 300 feet long by 138 feet wide. In addition to the fare collecting equipment located along the platform there are waiting rooms, toilet rooms, switch rooms and trainmen's room. An entrance and exit to Charles Street is provided for passengers on the north side of the station at the easterly passageway.

Plate VI shows the train and surface car platforms on the upper level.

The superstructure for the station was completed by A. G. Tomasello & Son, Inc., on August 20.

The waiting rooms, roofing, skylights, painting, etc., were done under contract with J. A. Singarella. These rooms are heated by electricity but will later be heated by steam upon the completion of the central heating plant on Faulkner Street.

The plastering of the passageways and stairway ceilings and walls also the bus platform wall was done under contract with Stephen T. Keith Company.

The roofing for the superstructure was done by T. J. Flynn Metal Works.

The plumbing for the station was done by Walsh & Brown.

Handrails on all stairways were furnished and installed by James A. Glass.

Wire gratings for the busway windows were furnished and installed by the Hub Metal Works.

Plate VII is a general view of Fields Corner Station showing train tracks and surface car ramps.

Plate VIII is a plan of Fields Corner Station.

MOVING CLAYTON STREET BRIDGE.

The layout for the new rapid transit tracks that were to cross the bridge over Clayton Street near Harrison Square necessitated moving this bridge about ten feet to the north of its old location in order to adapt it to the new track alignment. The old structure was a plate girder bridge of the half through type formerly carrying two steam railroad tracks. It spanned between concrete abutments with intermediate posts on the sidewalk curbs below. The weight of the bridge was about 110 tons. It was moved to its new position by the Department's ironworkers, the method used being as follows:

New concrete foundations with anchor bolts set therein were built to support the sidewalk posts in their new location. The seats in the concrete abutments were cut where necessary and prepared to receive the girders in their new position. The bridge was then jacked up by using twenty-five ton screw jacks, after which old rails were placed beneath the posts. These rails were thoroughly greased and the bridge was jacked along gradually to its new position after which it was lowered on to its new foundation and securely anchored. Some further alterations were then made to the stringers to provide for reinforced concrete floor which was later placed by the general contractor for Section Two.

Plate IX shows the bridge while being moved to its new position.

HARRISON SUB STATION.

A detailed description of this sub station was given in the last annual report. It was built in connection with the power distribution system for the new rapid transit extension. The construction of the building and transformer platform was well under way at the close of last year and the general contractor for this work, John P. Curley, completed his contract on April 14.

The installation of the rotary converter, transformers, switchboards, necessary wiring, etc., was done by the Boston Elevated Railway under their contract for installing the equipment furnished by the Department for the Dorchester Rapid Transit.

The wire grating protection over the transformers on the transformer platform was furnished and installed by Joshua Horricks, Inc.

Plate X is a view of the sub station located on Dickens Street near Harrison Square at the beginning of the Shawmut Branch.

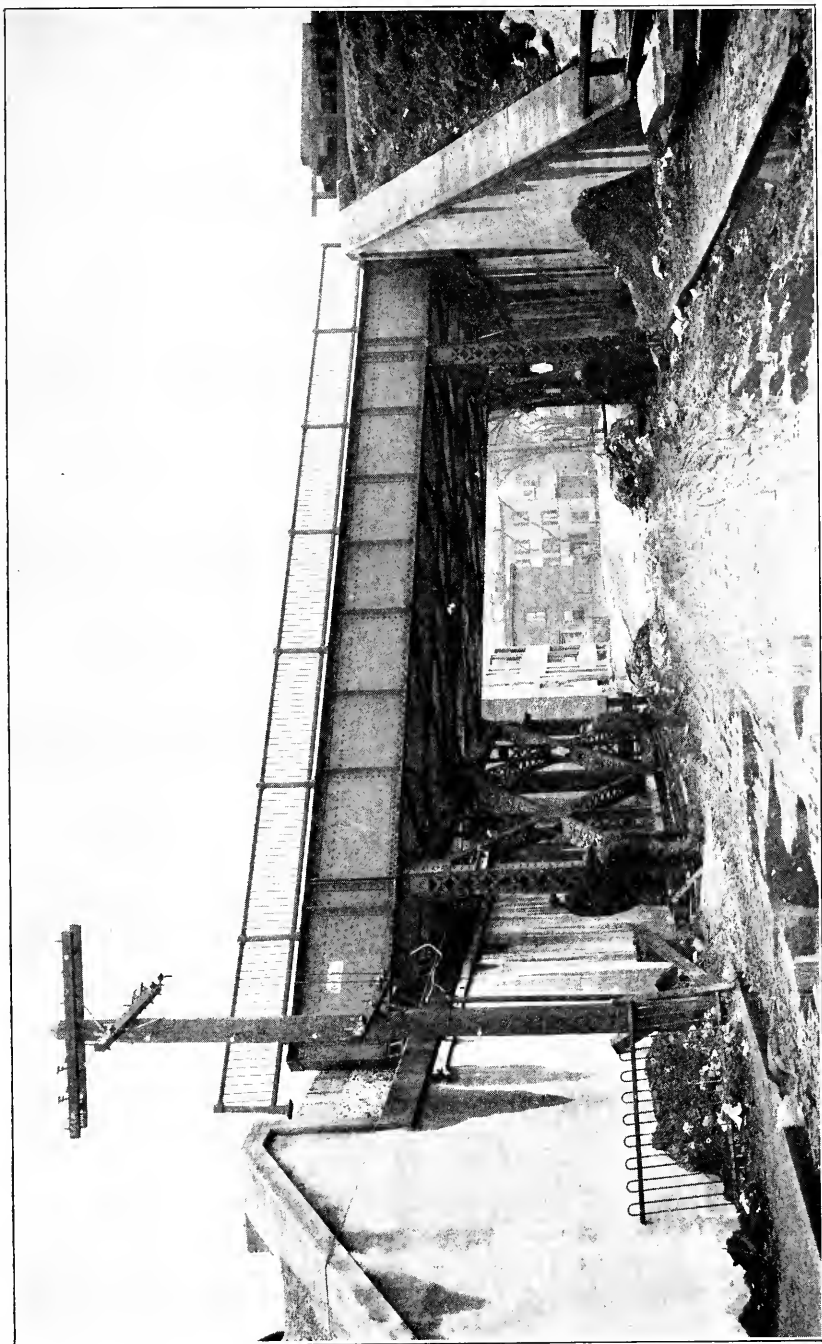
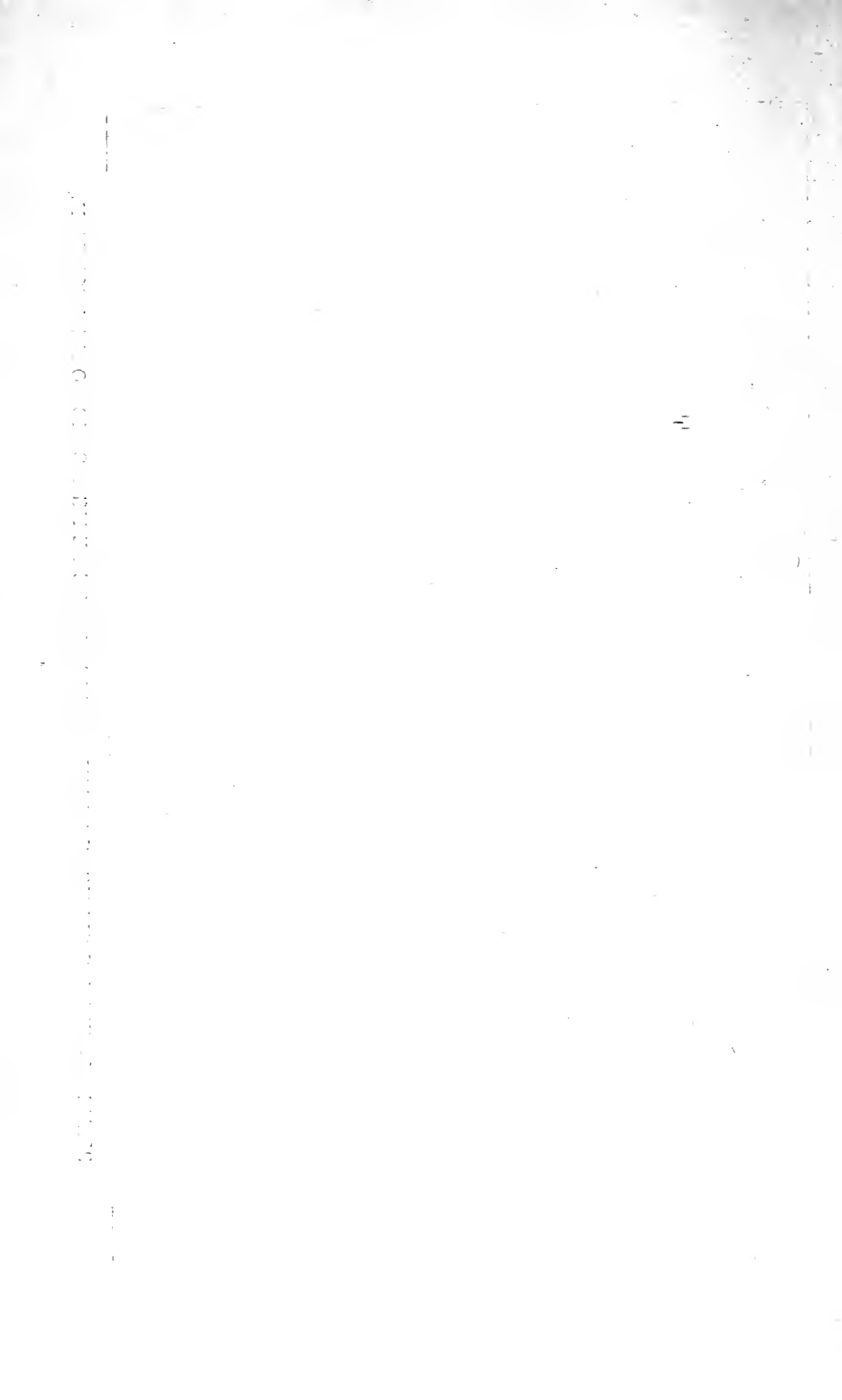


PLATE IX.—VIEW SHOWING CLAYTON STREET BRIDGE BEING MOVED TO NEW POSITION.



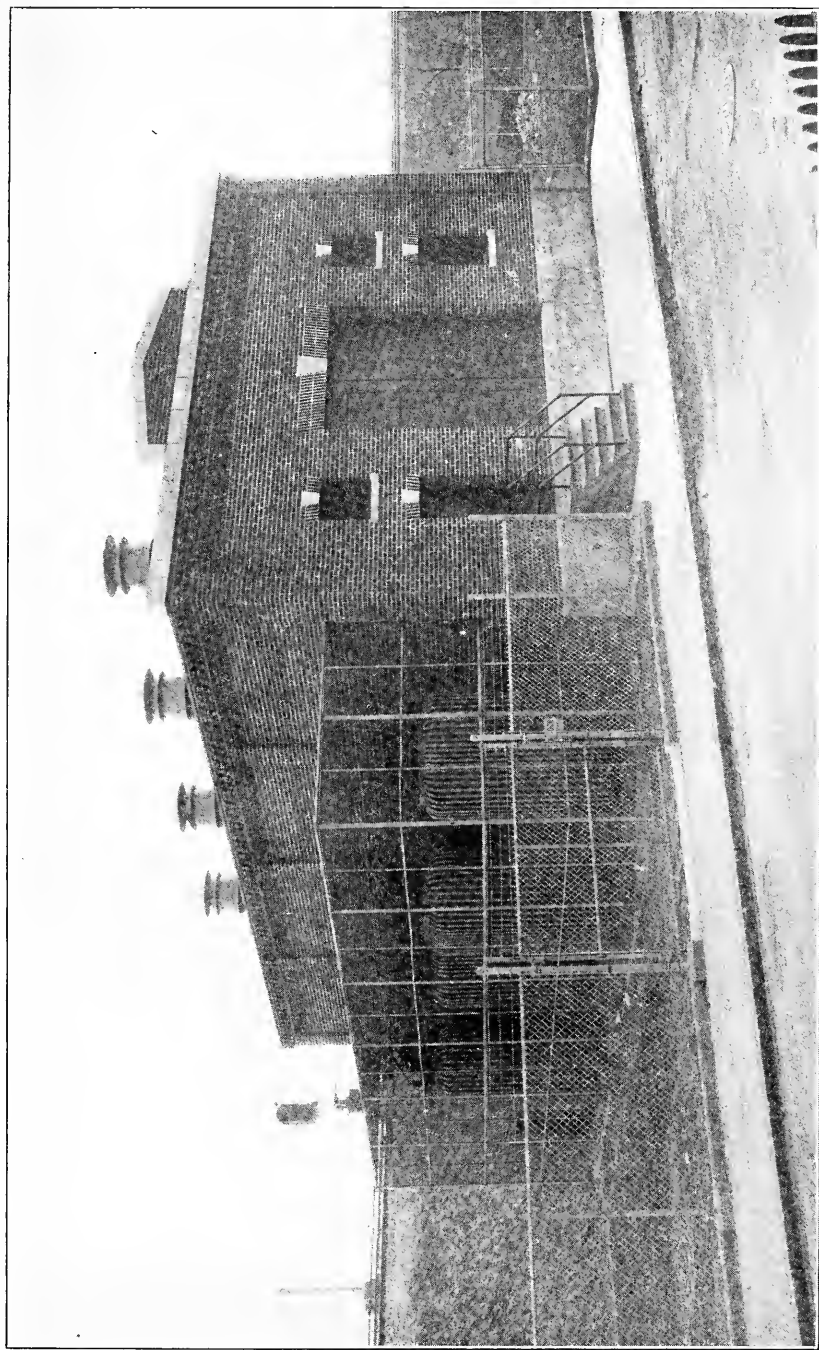


PLATE X.—HARRISON SQUARE SUB STATION.



Upon completion of the work on Section Two, the old Fields Corner railroad station which had been used by the Department's engineers as a construction field office, was demolished by the Lowell Building Wrecking Company.

OPENING NEW LINE TO FIELDS CORNER.

The date set for opening the new rapid transit extension from Andrew Square to Fields Corner, a distance of about two and one-half miles, was Saturday, November 5. On Thursday, November 3, His Honor, Mayor Malcolm E. Nichols, inspected the work and drove the last track spike at a point just west of the Fields Corner Station. On Friday the official inspection was made by representatives of the State and City together with invited guests. They were carried over the new line in special trains from Andrew Square Station. On Saturday morning the new line was thrown open to the public. Third rail trains are operated on two minute headway in the morning and evening hours and on a three minute headway throughout the rest of the day. The running time from Harvard Square to Fields Corner, a distance of about seven and one-third miles, is twenty-two minutes.

FIELDS CORNER ENCLOSED AREA AND HEATING PLANT.

A contract for an enclosed area with a bus terminal and a central heating plant was let on December 1 to the White Construction Company, the lowest of eleven bidders. The contract provided also for steam heating equipment for Fields Corner Station as well as the necessary piping for the enclosed area. Work on the foundations of the buildings was started early in the month. To date the excavation has been finished, concrete foundations built and the brickwork started.

The removal of several old buildings that were located on a portion of the site was done under two contracts; one with the Central Building Wrecking Company for eight of the buildings, and one with the Lowell Building Wrecking Company for one building. The work of removal was completed early in the fall.

SECTION THREE.

Construction work on this section was carried on in the vicinity of Park Street during the early part of the year. A cableway about 400 feet long running south from Park Street

was erected for the purpose of handling the excavation, also materials such as structural steel and concrete that were going into the new construction. Later on this cableway was moved further along and a second cableway was also used along the line of the work.

In the early spring work was started just south of Melville Avenue. The invert for Shawmut Station was completed and erection of steel for the station started. Excavation for the Welles Avenue pumpwell was completed and excavation was begun for the section from Peabody Square north toward Welles Avenue.

At the Peabody Square end of the section a tower was erected for the purpose of shooting concrete for the invert, sidewalls and roof. The tower was about 140 feet in height above the Peabody Square Bridge floor and the chutes reached a distance of about 320 feet along the line of the structure.

Plate XI is a view taken looking south at this location showing the tower and part of the covered section under construction.

As soon as the structure at Park Street was finished and the street opened for traffic the crossing at Melville Avenue was closed temporarily. The work throughout the entire length of Section Three, extending as it did for a distance of about one mile, was so arranged that not more than two of the five street crossings were temporarily closed to public travel at any one time. Pipe crossings were built under the sidewalks at all of these street crossings. Spaces were provided for gas pipes, water pipes, telephone and electric conduits.

The sewer changes required between Lyndhurst and Kenwood Streets were made during the spring. Two new sewer siphons were built crossing under the new covered section, one in the vicinity of Melbourne Street near Welles Avenue and the other in Centre Street. These siphons were built to replace the old sewers which crossed under the Shawmut Branch Railroad, and which were cut off by the new rapid transit structure. The Melbourne Street siphon is of a circular reinforced concrete section six feet in diameter with a U-shaped reinforced concrete bypass 24' x 18'. The Centre Street siphon is a cast-iron pipe forty-two inches in diameter with a cast-iron pipe bypass sixteen inches in diameter, both pipes surrounded with concrete. At each end concrete manholes are provided for proper access to the siphons.

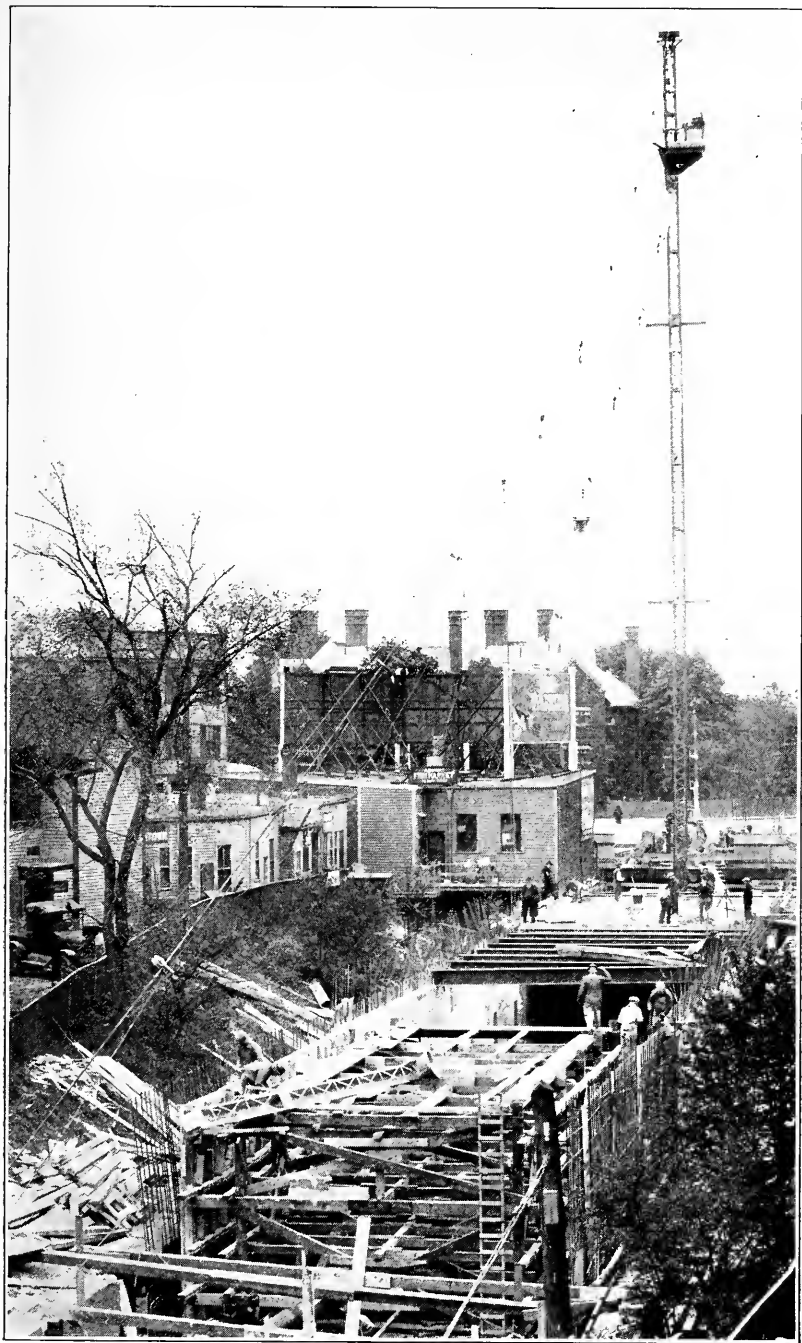


PLATE XI.—CONSTRUCTION, SECTION 3, JUST SOUTH OF
PEABODY SQUARE, ASHMONT.



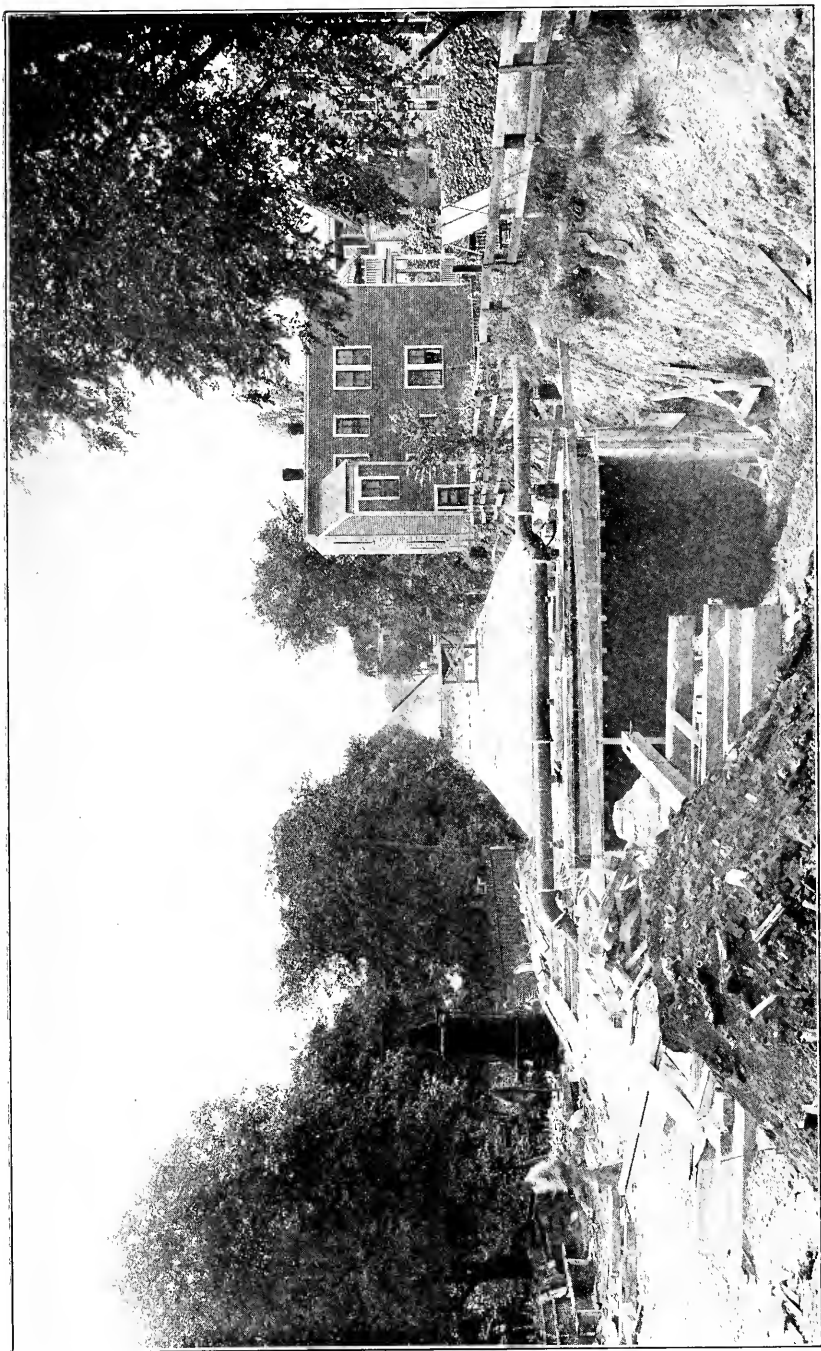


PLATE XII.—GENERAL VIEW ALONG ROOF OF COVERED SECTION.

A general description of Section Three was given in the last report. The work embraced in the general contract was substantially completed during October.

Plate XII is a typical view taken along the roof of a portion of Section Three.

Upon completion of the general contract for Section Three, construction was immediately started on the entrance and exit building for Shawmut Station.

SHAWMUT STATION.

The entrance and exit building is being built directly over the center of the underground station. It is located opposite the site of the old Shawmut railroad station and is to be a one story brick building not unlike in general type of design the stations at Columbia and Savin Hill. The exterior is of tapestry brick with brownstone capping. The roof is of steel and reinforced concrete, the ceilings are plastered and the interior walls are faced with white enamelled brick. Toilet rooms and a stationmaster's room are provided.

The building is so arranged that passengers may reach either of the lower side platforms by two combined entrance and exit stairways one located at each side of the building. The structure is under contract with White Construction Company and the work is now at the stage where the plastering, painting and granolithic work are proceeding. The plumbing contract has been awarded to the Downey Company and installation of piping and plumbing fixtures has been started.

The side platforms of the station below, while designed for an ultimate length of 435 feet, are to be finished at present for a length of 300 feet, this being the present standard length of the other rapid transit station platforms.

The contract for plastering the walls and ceilings has been let to George Craffey Company and work is now in progress.

Contracts for granolithic platforms and also for hand-rails, doors and other equipment will be let as soon as the plastering is far enough along to permit the work to proceed.

EQUIPMENT.

The installation of equipment such as tracks, lighting, power conduits, etc., for this section has followed closely the work of the general contractor. The laying of ties, rails and the ballast-

ing of the tracks has been carried along from the portal of the covered section, southerly toward Shawmut Station.

In the Welles Avenue pump wells two centrifugal pumps have been installed. The pumps were furnished under contract with Starkweather and Broadhurst Company. Each pump has a capacity of 400 gallons per minute. One pump is a horizontal pump mounted on a platform and driven by a 600-volt General Electric motor. The second pump is a vertical submerged pump connected by a shaft to a motor of similar type. The installation of the pumps was completed and the pumps satisfactorily tested early last month.

SECTION FOUR.

This section extends from the north side of Peabody Square Bridge to a point near the Shawmut Junction of the old Shawmut Branch railroad. It is about three quarters of a mile in length and includes the Ashmont Station, the Codman Street Yard and a portion of the high speed trolley right of way beyond Ashmont.

The work in the general contract consists of the construction of a roadbed, station platforms, approaches, viaducts and passageways, sewers, drainage systems, concrete retaining walls, foundations, etc., concrete roadways and sidewalks, bridges, certain equipment such as conduit lines, manholes, etc., and the removal of structures, repaving, regrading, etc.

The Ashmont Station is to be the present terminal for the third rail rapid transit service of the Cambridge-Dorchester Tunnel. It is to be just south of Peabody Square and the entire layout including approaches, viaducts, busway, entrances, etc. covers a triangular shaped area of about five acres.

In addition to land acquired by the purchase of the Shawmut Branch from the New York, New Haven & Hartford Railroad it was necessary to take by eminent domain twenty-three parcels of real estate including sixteen dwellings and three stores. The takings were made early in the spring. Many of the buildings were moved to other locations and several were demolished under various contracts during the summer in time to permit construction work on the new station to start in the early fall.

The work of preparation at the Codman Street Yard, mentioned in the last annual report, had proceeded to a point where over one half of the filling had been placed and all of the build-

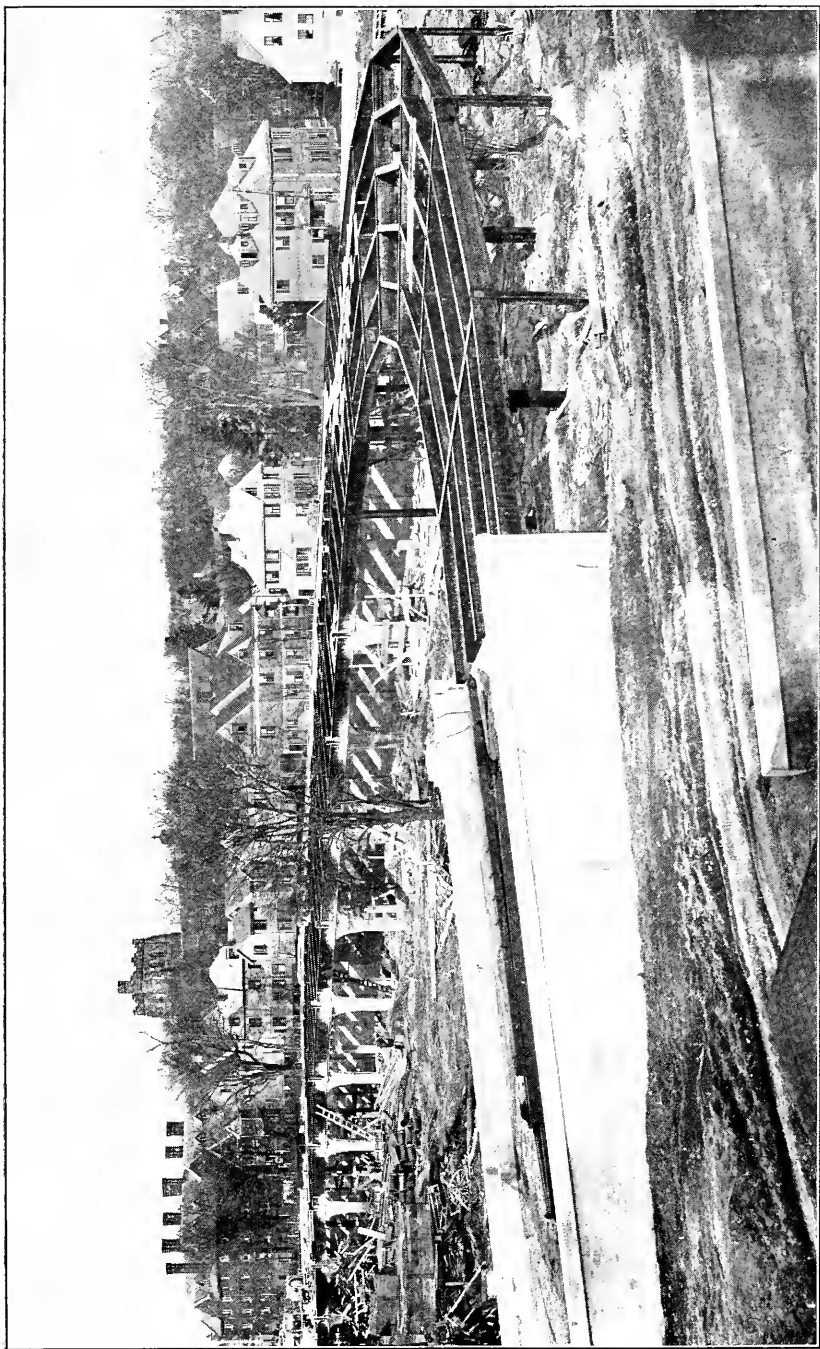


PLATE XIII.—STEELWORK FOR BUSWAY, ASHMONT STATION.



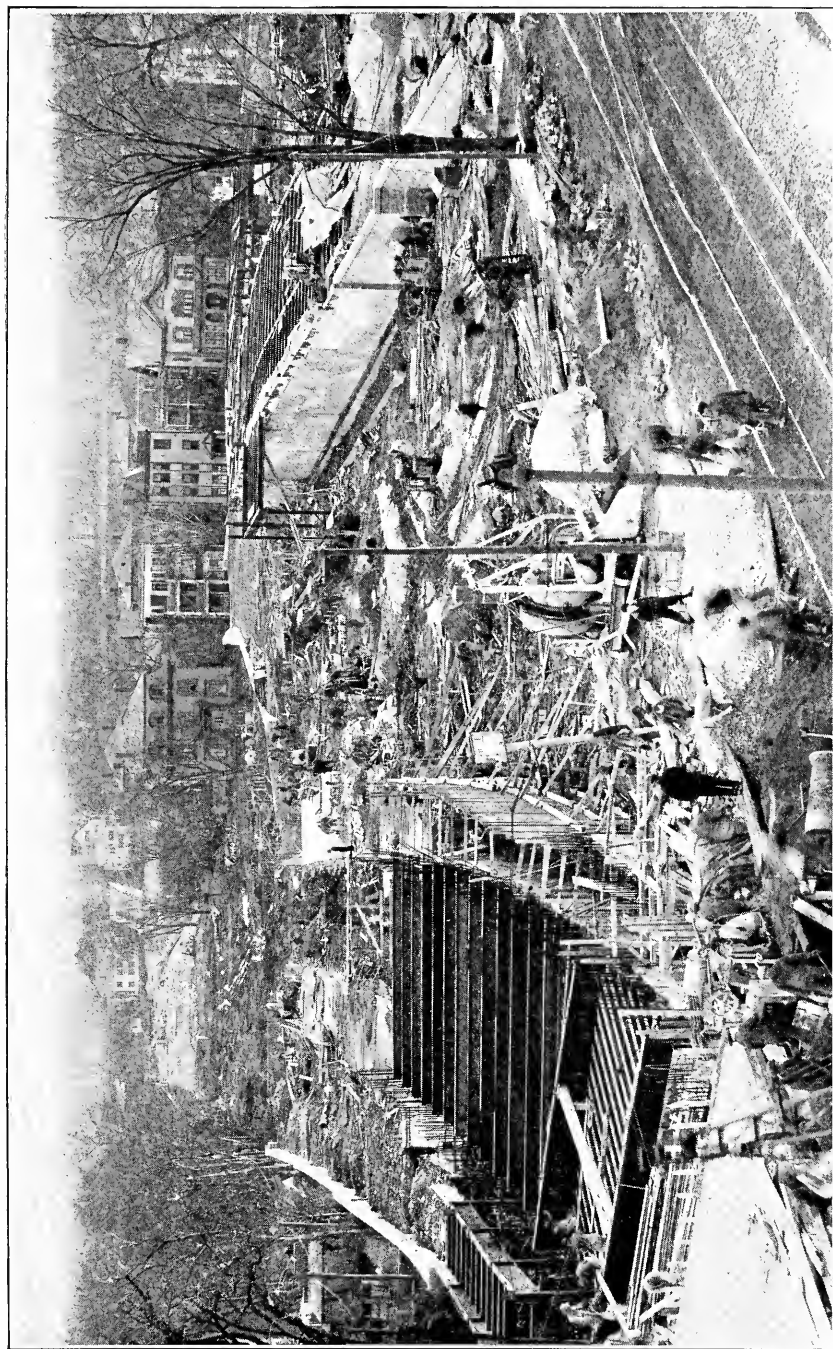


PLATE XIV.—GENERAL VIEW CONSTRUCTION, ASHMONT STATION.



ings removed with the exception of one. This building was an old wooden structure used as a creamery and was torn down during the month of April under contract with the Lowell Building Wrecking Company.

The general contract for Section Four was let September 16 to the C. & R. Construction Company, the lowest of eight bidders. Work was started in September at the north end of the Peabody Square Bridge. The old railroad bridge under Peabody Square was built with granite masonry abutments supporting a wooden bridge floor. The length of the old bridge across the square was about three hundred feet. In starting the work about one-half of this distance was closed to traffic, the old flooring was then removed and the old abutments trimmed back where necessary to widen the cut. The new reinforced concrete invert and sidewalls were then carried along in about fifty foot sections after which the new steel bridge floor beams were placed, the reinforced concrete floor built and temporarily paved with granite blocks. This method was followed across the square leaving at all times spaces open for traffic equal in all to about one-half the entire length of the bridge. The Boston Elevated Railway relaid with new rails and ties the two surface car tracks which crossed diagonally over the bridge.

Work on this general contract was also started on the foundations for the Ashmont Station including retaining walls and column footings for station train shed, busway shelter, busway and trolley car viaducts. The busway steel has been erected and partly encased at this date.

Plate XIII shows a view of the steelwork for the busway floor.

Plate XIX is a general view looking south from Peabody Square and shows the work of the Ashmont Station at this stage of the construction. The covered section just south of the Peabody Square Bridge is shown on the left of the photograph. On the right is the busway and the space between will be occupied by the station proper in which there will be two rapid transit and four surface car tracks.

ASHMONT SUB STATION.

An automatic sub station for the power distribution system will be built just south of the Ashmont Station. The sub station will face on Beale Street and is similar to the Harrison

Square Sub Station, the design of which together with a detailed description of its equipment may be found in the last annual report.

The contract for this building was let to the White Construction Company, the lowest of seventeen bidders. Work was started the latter part of November and to date the excavation has been completed and the basement walls and floor have been built and waterproofed.

CODMAN STREET BRIDGE.

Construction of the bridge over Codman Street on the location of the old Shawmut Branch Railroad Bridge, which was started last year, was continued during the winter and summer of the current year and the bridge completed in the month of September. This bridge, described in the last report, was built under an agreement made by this Department with the Massachusetts Department of Public Works in connection with the building of the Southern Artery. Its completion permits the installation of the signal and power conduits for the rapid transit lines which are to be carried through from Ashmont into the Codman Street Yard. Upon completion of the contract work on Section Four the new trackwork for third rail and trolley lines can be carried across without interruption.

TREMONT STREET SUBWAY.

REMOVAL OF OLD SCOLLAY SQUARE ENTRANCE AND EXIT BUILDING.

The removal of the old structure over the main entrance and exit stairway at Scollay Square, together with the substitution of a much smaller covering, resulted from legislation which provided for the widening of Cambridge Street into Scollay Square. The traffic at this part of Scollay Square is most congested and the old building obstructed the vision in many directions.

Several studies were made to determine the type of structure that would best replace the existing building and at the same time be low enough to give a clear view of all parts of the square and street intersections. The method of procedure was also studied as the stairway itself is in constant use and there are no other stairways that could be used without serious inconvenience and delay to the public. It was decided there-

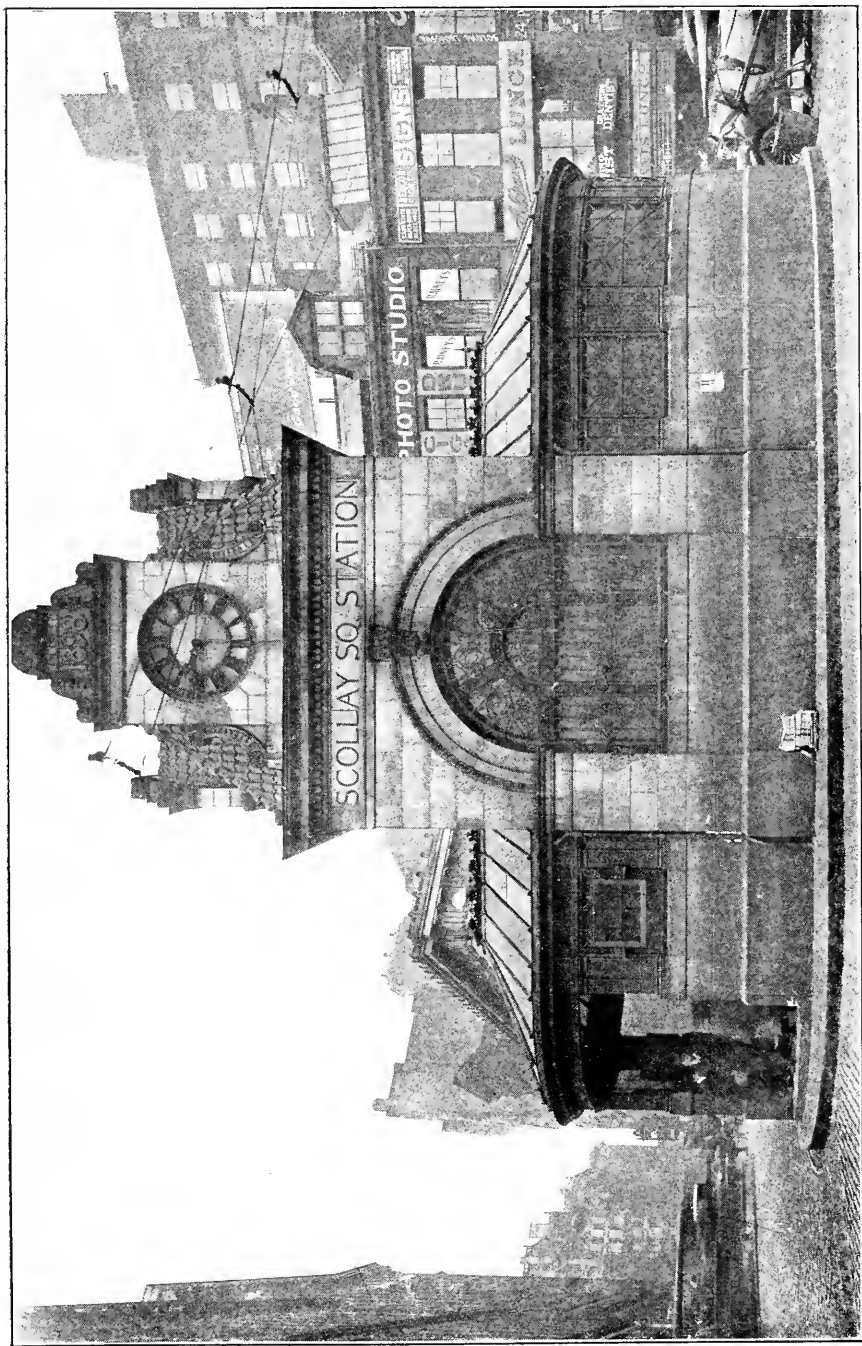


PLATE XV.—OLD STRUCTURE OVER STAIRWAY, SCOLLAY SQUARE STATION.

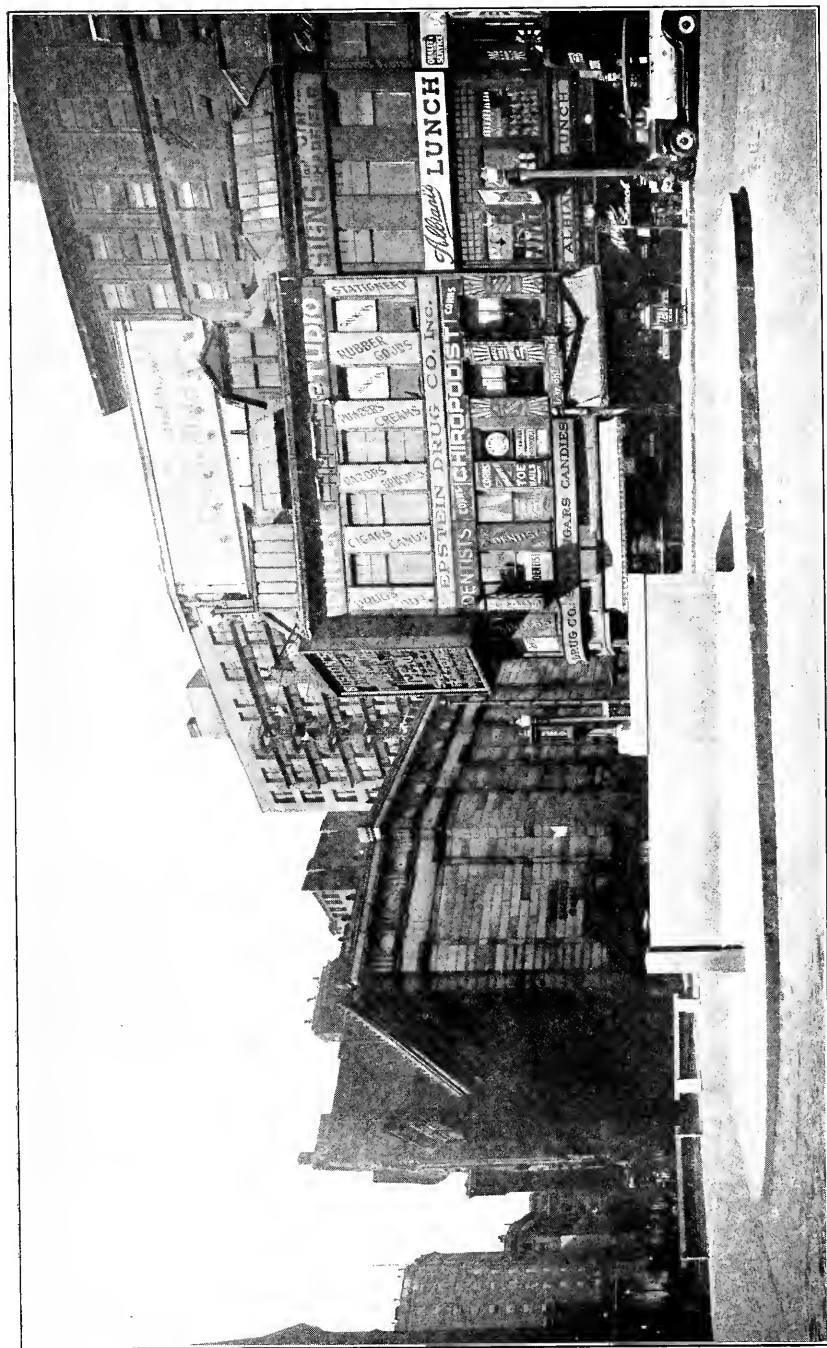


PLATE XVI.—NEW STRUCTURE OVER STAIRWAY, SCOLLY SQUARE STATION.



fore to provide in the contract for the removal of the building, that all work should be done at night, leaving the stairs in a clean and safe condition during the day time. As the removal of the building left a large open area, a temporary shelter was erected large enough to cover the area and provide room in which to work without interfering with traffic. A space in the rear, or to the north, of this shelter was enclosed by a tight board fence and gate providing a small yard for receiving and storing materials. The shelter was erected and removed later by employees of the Department, as was also the work of changing over the structural steel framing, removing old masonry, placing the new concrete, waterproofing, granolithic landing, etc.

The tearing down of the old building was done during the month of October under contract with the Central Building Wrecking Company.

The polished granite sides of the new shelter were furnished and erected by M. Solimando.

Interior plastering was done by A. V. Day Company.

The illuminated sign was furnished and erected by the Flexlume Corporation.

The regrading and repaving on the east side of the street from Cornhill to a point near Brattle Street was done by Samuel J. Tomasello.

The work incidental to the drainage system was done by T. J. Flynn Metal Works.

The entire work was completed in December.

Plate XV shows the old structure over the stairway just prior to its removal.

Plate XVI is a view taken from the same spot and shows the new structure built over the same stairway.

CYPHER STREET YARD, SOUTH BOSTON.

The work in the Cypher Street Yard and steel shop has continued this year as for several years past under the direction of Assistant Engineer Samuel C. Lyman.

All construction supplies and materials that are furnished by the Department have been received and distributed from this yard.

Most of the structural steel used during the year has been shipped from the mills direct to the yard where it has been fabricated in the steel shop by the department's ironworkers.

The handling of cement for testing has been done by the laborers from the yard. These men have also done necessary grouting work in the subways and tunnels during the night time when trains are not running, as well as the alteration work carried on nights at Scollay Square as before mentioned.

TESTING MATERIALS.

The inspection of all construction work has been done by the Department's inspectors assigned to the various sections of the work. In addition to the field inspection the materials have been tested as in former years at the mills and in the laboratories from samples taken from the field.

Reinforcing rods, structural steel, rails and castings have been tested by the firm of Conard & Buzby. Chemical analysis of cement has been made by the firm of Skinner, Sherman & Esselen, Inc. Physical tests of samples of every lot of cement received have been made by Mr. Charles N. Ryan, Cement Tester, Public Works Department. Chemical and physical tests of waterproofing fabric and asphalt have been made by Mr. Hiram Y. Waterhouse, Chemist, Public Works Department.

ENGINEERING FORCE.

The names of those members of the Engineering Division employed for more than one month are given in Appendix II.

Respectfully submitted,

ERNEST R. SPRINGER,

Chief Engineer.

APPENDIX I.

CONTRACT BETWEEN THE CITY OF BOSTON AND
THE BOSTON ELEVATED RAILWAY COMPANY
FOR THE USE OF DORCHESTER RAPID
TRANSIT.

I. This contract made this seventeenth day of October in the year one thousand nine hundred and twenty-four by and between the City of Boston, hereinafter called the city, acting by the Transit Department, hereinafter called the department, under and by virtue of an act of the Commonwealth of Massachusetts, entitled "An Act Providing for the Extension of Rapid Transit Facilities in the Dorchester District of the City of Boston," being chapter four hundred and eighty of the acts of the year one thousand nine hundred and twenty-three, hereinafter called the act, and the Boston Elevated Railway Company, hereinafter called the company, witnesseth as follows:

II. The city, pursuant to the act and in consideration of the covenants and agreements herein contained, grants to the company the use of the premises as defined in the act, and to be acquired, laid out and constructed under authority thereof, and in general in accordance with the plans prepared by the department numbered 14212, and contained in sheets 1 to 10, inclusive, and entitled City of Boston — Transit Department Plan showing proposed extension DORCHESTER RAPID TRANSIT FACILITIES as required by section 4, chapter 480 of the Acts of 1923 and such changes as may be made therein in accordance with the act, and Article XXII. hereof, and of the equipment as defined in the act, and to be provided and furnished under authority thereof, for the running of its trains and cars therein and thereon and such other uses as are hereinafter specified.

III. The use shall begin upon certification by the Department of Public Utilities that said premises and equipment are in safe and proper condition for operation.

IV. The term of years of the use shall extend from the beginning of the use of the premises and equipment to the expiration or termination of the present lease of the Dorchester tunnel.

V. The rental shall be at the rate of four and one-half per cent per annum upon the fair and reasonable cost, as deter-

mined by the Department of Public Utilities, of the premises and equipment; provided, however, that the annual rental shall be sufficient to provide for an amount equal to one-half of one per cent of said cost in addition to the annual amount of interest on the bonds issued to pay for said cost, but not less than said four and one-half per cent in any event.

The cost of the premises and equipment shall be deemed to include, except as otherwise provided in the act, all expenditures incurred in acquisition and construction, including damages, expenses, such proportion of the salaries of the department as may in its opinion be properly chargeable thereto, and interest on the debt incurred for the acquisition and construction of the premises and equipment prior to the beginning of the use. For the purpose of ascertaining the rental there shall be deducted from the cost of the premises and equipment the proceeds of sales and leases of land or rights or interests in land or other property acquired, provided or furnished by the department for the purposes of the act, and the proceeds of sales or leases of buildings or other structures upon lands and of equipment so acquired, provided or furnished, and the fair valuation of any such lands or other property or equipment no longer needed for the purposes of the act but not actually sold, as agreed upon by the department and the company, or, in case of difference, as determined by the Department of Public Utilities. Any interest received by the city upon the proceeds of the bonds issued by it prior to the expenditure of such proceeds shall be credited against interest during construction in ascertaining the cost.

The rental shall begin when the use begins.

The rental shall be paid to the city in quarterly payments on the last day of December, March, June and September in each year and at the rate aforesaid for any uncompleted quarter of a year.

If at any time during the continuance of the term of this contract the company shall be deprived in whole or in part of the use of the premises or of said Dorchester Tunnel or the Extension thereof to be constructed under authority of the act, so as to prevent the practicable use of the whole or a part of the premises for operation in connection with said Dorchester Tunnel by any cause not due to any act of the company, or its agents, servants or licensees, in the use of the premises and equipment, or to any negligence on its or their part, or to any failure of the company to maintain the premises and equip-

ment in good order and condition as herein provided, then the rental or a just and reasonable part thereof, as agreed upon by the mayor of the city and the company or in case of difference as determined by arbitration as hereinafter provided, shall be suspended or abated during such deprivation.

VI. The company shall maintain the premises, except as to repairs below excepted, in good order and condition as a structure complete so far as consistent with the provisions of the act, and adapted to the maintenance and use of lines of railway, and shall at all reasonable times be entitled to a permit to open the streets and other public grounds of the city for the purpose of making requisite repairs to the premises, and when the right of the company or its assigns to use the premises shall terminate shall restore them to the city in good condition except as to repairs not obligatory upon the company.

All repairs to the premises shall be at the sole cost and expense of the company except such repairs as are made necessary by any cause not due to any act of the company, or its agents, servants or licensees, in the use of the premises and equipment, or to any negligence on its or their part, or to any failure of the company to maintain the premises and equipment in good order and condition as herein provided; or growing out of the location, maintenance or use of the wires or other apparatus which the city is hereinafter authorized to maintain in the premises; and if such repairs shall be made necessary by any of said excepted causes then such repairs shall be made by the company in a manner satisfactory to the department, and the reasonable cost and expense thereof deducted from the rental subsequently payable.

VII. The city shall not be responsible to the company for damages of any description resulting from any defects in the premises or equipment, whether structural or arising out of want of repair or from any cause after the use of the same by the company has begun as hereinbefore provided, unless such damage result from the location, maintenance or use of the wires or other apparatus which the city is hereinafter authorized to maintain in the premises; nor shall it be responsible for any damages resulting to persons or property in the operation and use of the premises and equipment, including all parts thereof, whether on property belonging to the city or upon property the fee of which belongs to other parties, and the company shall hold the city harmless and indemnified therefrom and shall at its own expense upon due notice from the city defend all suits

and other proceedings of every description, whether at law or in equity, which may be brought against the city, its officers, servants, or agents by reason of any liability arising out of the operation and use of any portion of the premises and equipment or of the railways, machinery and apparatus therein and accruing after the right to use such portion has begun as herein provided, and shall satisfy all final judgments of legal tribunals rendered in such suits and proceedings. The foregoing provisions shall not be construed to impose any liability or obligation upon the company for any cause not due to any act of the company, or its agents, servants or licensees, in the use of the premises and equipment, or to any negligence on its or their part, or to any failure of the company to maintain the premises and equipment in good order and condition as herein provided; or growing out of the location, maintenance or use of the wires or other apparatus which the city is hereinafter authorized to maintain in the premises.

VIII. The Company shall keep the premises clean and suitably lighted and the approaches to the stations free from snow and ice.

IX. The company within the limitations of the act may make such alterations in or additions to the premises as may be approved by the department.

X. The company shall maintain and keep the equipment furnished by the department as provided in the act in proper repair and condition during the term of this contract, and all necessary replacements, substitutions and renewals shall be provided, furnished and maintained by the company and shall be of at least equal character, quantity and value with that replaced, substituted for or renewed. All repairs in respect of all equipment shall be made by the company at its sole cost and expense. In case of any neglect of the company to maintain all equipment and to provide and furnish repairs, replacements and renewals as required by the terms of this contract, the department may, upon reasonable notice, require such maintenance, provision and furnishment to be made; and, upon the failure of the company to conform to such requirement forthwith and upon such notice, then the department may provide and furnish the same; and for such purpose the department shall, so far and for such time as may be necessary, be entitled to enter upon any part of the premises or equipment, and the company shall forthwith reimburse the city for all cost and expense reasonably incurred therefor.

The department shall furnish the company for its examination within six months after the date of the beginning of the use of the premises and equipment a true schedule in detail as to material and cost of the equipment furnished for the initial use of the equipment by the company. After the use of the premises and equipment shall have begun the company shall, within thirty days after the expiration of each year of use, file with the department a true schedule in detail of all repairs, replacements, substitutions and renewals provided and furnished by it during such year. When the right of the company or its assigns to use equipment furnished by the department shall terminate, the company shall restore it to the city in good condition except as to repairs not obligatory upon the company.

XI. To the extent of the power of the company so to do and the power of the department to contract therefor, the company may place and maintain in the stations and shelters booths for the sale of newspapers, magazines, periodicals and books, and in places therein specially adapted therefor may place or admit unobjectionable advertisements, and may make such other uses of the premises, not impairing the use for transportation of passengers, as it may from time to time determine; provided, however, that such use or uses shall not diminish or impair the safety, accommodation, convenience or comfort of passengers using the premises; and the company agrees that upon receipt of notice in writing at any time or from time to time from the Department of Public Utilities that in its opinion any of such uses, either in whole or in part, in any way diminish or impair such safety, accommodation, convenience or comfort or conflict in any way with the best interests of the public, it will forthwith to the extent specified in the notice discontinue such use.

XII. The company, upon such terms as it may deem expedient, may permit any person or corporation not authorized to carry on a railway business but authorized to use and maintain wires, conduits, tubes or similar structures along the route of the premises, to place such wires, conduits, tubes or similar structures within the premises used by the company, but only to such extent and for such time as may be practicable without interfering with the safe and convenient operation of the railway and other apparatus which the company is hereby authorized to put therein.

XIII. The city may place in the premises such wires and apparatus as may be necessary for its police and fire alarm

service, to be used however exclusively for such service and to be so located as not to interfere with the use of the premises which the company is hereby authorized to make. The location, construction, maintenance and repair of such wires and apparatus shall be subject to such reasonable directions and regulations as the company may impose or in case of any disagreement as the Department of Public Utilities may determine.

XIV. In the event of the failure of the company or its assigns to pay the rental for three months after such rental shall have become due, or in the event of a failure to maintain or operate a railway in and on the premises, and if such failure shall have continued for three months, then in either of said events the city upon three months' notice, such default still continuing, shall have the right to terminate this contract and to re-enter upon and repossess itself of the premises and equipment, unless such failure to maintain and operate grows out of any cause not due to any act of the company, or its agents, servants or licensees, in the use of the premises and equipment, or to any negligence on its or their part, or to any failure of the company to maintain the premises and equipment in good order and condition as herein provided.

XV. The company shall have no right at any time to remove from the premises any equipment necessary to the use and maintenance of the premises and the operation of a railway therein, except for the purpose of repairs, replacements, substitutions, renewals and additional equipment.

XVI. The governor of the commonwealth, the mayor and commissioner of public works of the city, and the members of the Department of Public Utilities and of the department, and their respective engineers, shall at all times have free entry to the premises for the purpose of inspecting the same.

XVII. In case of disagreement between the city and the company on any matter as to which the method of arbitration has not been hereinbefore provided for, the matter in dispute shall be left to the decision of three persons, one to be selected by the mayor of the city, one to be selected by the company, and the third by the two thus chosen. The report of the arbitrators or a majority of them shall be binding upon the parties hereto.

XVIII. In respect of all matters arising under this contract where provision is made for action by the department or its approval of acts to be done by the company is required,

it is provided and agreed that upon the termination of the existence of the department the authority to take such action shall vest in the city, which shall have the rights, powers and privileges and be subject to all the duties, restrictions and liabilities herein conferred or imposed upon the department in respect thereof; such powers to be exercised by the mayor, commissioner of public works and city treasurer in place of the department or by such other officers as the city council may prescribe.

XIX. With respect to the equipment, use and operation of the railway to be located in the premises and transportation thereon, the company is to have all the powers and privileges and be subject to all the duties, liabilities, restrictions and provisions set forth in general and special laws which now are or hereafter may be in force applicable to it.

XX. This contract shall not in any respect impair any right which the commonwealth or the city, or any other licensee of the commonwealth may at any time have to take the railway properties of the company, in the event of such taking the compensation to be paid to the company shall not be enhanced by reason of this contract nor shall it be diminished because of the fact that without this contract the connection between different parts of said properties might be cut off.

XXI. All provisions of law which are required to be made a part of this contract are hereby incorporated by reference and made a part hereof, and it is understood and agreed that anything herein contained which is contrary to or inconsistent with the provisions of the act is and shall be void and of no effect.

XXII. No changes in the plan required under the act to be filed with the commissioner of public works of the city and submitted to the company for its examination shall be made without the consent of the company thereto in writing.

XXIII. Upon the final termination of the use of the premises under this contract or any extension or renewals thereof, prior to the extinguishment of the debt incurred therefor the company shall recompense the city for any depreciation then existing in the equipment, as agreed upon between the department and the company or in the event of their failure to agree as determined by the department of public utilities or such board or commission as may then be vested with the jurisdiction in general at present vested therein with reference to street railways; the company to be charged with that

fractional part of the original cost to the city of each item, which the elapsed life thereof bears to the total estimated life and, so far as any renewals, substitutions or replacements exceed in cost to the company the original cost to the city, to be credited therefor on a like basis for the remaining life thereof. In such determination the total estimated life shall consist of the elapsed life plus the length of time which it is estimated that the item in question will be properly usable.

IN WITNESS WHEREOF the parties hereto set their hands and seals the day and year first above written, the City of Boston acting by the department, pursuant to a vote of the department, its members not being bound in their personal capacity, and the Boston Elevated Railway Company, pursuant to a vote of the board of Trustees of the Boston Elevated Railway Company, causing its name and corporate seal to be affixed to these presents by its treasurer thereto duly authorized.

THE CITY OF BOSTON

by the

(SEAL)

TRANSIT DEPARTMENT OF
THE CITY OF BOSTON.

Signed by:

THOMAS F. SULLIVAN

FRANCIS E. SLATTERY

LOUIS K. ROURKE

BOSTON ELEVATED RAILWAY COMPANY

By HENRY L. WILSON, Treasurer (sgd)
(SEAL)

APPROVED AS TO FORM

Joseph P. Lyons (sgd)

Asst. Corporation Counsel.

COMMONWEALTH OF MASSACHUSETTS.

SUFFOLK ss.

BOSTON, October 17, 1924.

Then personally appeared Thomas F. Sullivan, Chairman of the Transit Department of the City of Boston, and acknowledged the foregoing instrument in the name and behalf of the said department to be the free act and deed of the City of Boston.

Before me,

EDWARD F. CONDON (sgd)
Justice of the Peace.

My commission expires
July 27, 1928.

A form of contract for the use of the premises and equipment to be acquired, laid out and constructed under authority of chapter four hundred and eighty of the acts of the year one thousand nine hundred and twenty-three entitled "An Act providing for the Extension of Rapid Transit Facilities in the Dorchester District of the City of Boston," and in general in accordance with the plan prepared by the Transit Department of said city numbered 14212, and contained in sheets 1 to 10, inclusive, and entitled City of Boston — Transit Department Plan showing proposed extension DORCHESTER RAPID TRANSIT FACILITIES as required by section 4, Chapter 480 of the Acts of 1923, having been presented to the meeting it was —

Voted: That Henry L. Wilson, Treasurer, be authorized in the name and on behalf of the Boston Elevated Railway Company to sign, seal, acknowledge and deliver said contract.

A true copy of vote adopted by the Board of Trustees of the Boston Elevated Railway Company duly held on the third day of September, 1924.

Attest:

STANLEY R. MILLER (sgd)

Member of Board.

Contract 780

APPENDIX II.

Twelve months ending December 31, 1927.

The names of those who have been employed in the Engineering Division for more than one month during the period covered by this report are given below, together with an indication of the principal work upon which they have been engaged.

WILBUR W. DAVIS, *Assistant Chief Engineer*. In general charge of construction.

LEONARD B. HOWE, *Designing Engineer*. In general charge of designing.

Assistant Engineers.

THOMAS N. ASHTON. Designs and details for structural steel, Dorchester Rapid Transit.

JOHN A. BERRIGAN. Track alignment calculations, land takings, detail plans, etc., Dorchester Rapid Transit.

THOMAS A. BERRIGAN. Plans and designs for reinforced concrete and steelwork, Dorchester Rapid Transit.

RALPH F. BOUDREAU. Lines and grades, Section 3; Surveys, Section 5, Dorchester Rapid Transit.

JAMES D. BURNS. Plans and designs for steelwork, Dorchester Rapid Transit.

HARRY T. CARROLL. Track alignment calculations, land takings, detail plans, etc., Dorchester Rapid Transit.

STANLEY J. CLIFFORD. Track alignment calculations and detail plans, Dorchester Rapid Transit.

JOHN J. CUMMINGS. Track alignment calculations and detail plans, Dorchester Rapid Transit.

LESTER S. DANIELS. Supervision of lines, grades and estimates, Sections 1, 2, 3 and 4, Dorchester Rapid Transit.

JOSEPH P. DEVER. Studies, specifications and estimates for Dorchester Rapid Transit. In charge of paving contracts and alterations Scollay Square.

HENRY L. DONOVAN. Taking plans and steel details for Dorchester Rapid Transit.

ROBERT B. FARWELL. Supervision of construction of Sections 2 and 4, Dorchester Rapid Transit.

RALPH A. FISHER. Designs and details of structural steel, Dorchester Rapid Transit.

LOUIS J. HARRIGAN. Track alignment calculations, taking plans, studies and detail plans for sewers for Dorchester Rapid Transit.

HERBERT D. HURLEY. Track alignment calculations, taking plans, design and details for structural steel for Dorchester Rapid Transit.

JOHN M. KENNEY. Plans and details for Dorchester Rapid Transit.

WILLIAM W. LEWIS. Supervision of construction, Section 1, Dorchester Rapid Transit.

BENJAMIN A. LOVELAND. Designs and details for structural steel and reinforced concrete structures, Dorchester Rapid Transit.

SAMUEL C. LYMAN. In charge of stockyard and steel shop at Cypher Street.

ARTHUR V. LYNCH. Supervision of track alignment calculations, taking plans, designs and detail plans for Dorchester Rapid Transit.

HARRY H. LYNN. Plans and details, Dorchester Rapid Transit.

* PATRICK F. O'BRIEN. Estimates and specifications for Dorchester Rapid Transit.

HARRY F. SAWTELLE. Supervision of designs and details for structural steel and reinforced concrete structures, Dorchester Rapid Transit.

HERBERT R. STEARNS. Supervision of track alignment calculations, taking plans, designs and detail plans for Dorchester Rapid Transit.

LEO S. STONE. Supervision of escalator alterations. Studies, estimates and specifications for construction, Dorchester Rapid Transit. Supervision of specifications and contracts for equipment, Dorchester Rapid Transit.

EDWARD SULESKY. Designs and details for structural steel and reinforced concrete structures for Dorchester Rapid Transit.

ARTHUR W. VOSE. Track alignment calculations, taking plans, detail plans, etc., Dorchester Rapid Transit.

DAVID B. WEDEN. Track alignment calculations, designs and details for structural steel and reinforced concrete structures for Dorchester Rapid Transit.

Assistant Engineer and Inspector.

FREDERICK C. H. EICHORN. Supervision of construction of Section 3, Dorchester Rapid Transit.

Draftsmen.

EDMUND A. BARRETT. Lines and grades, Sections 1, 2, 3 and 5, Dorchester Rapid Transit.

BORIS BERESTNEFF. Plans and details for steelwork, Dorchester Rapid Transit.

ROBERT D. GARDNER. Plans and details for steelwork. Lines and grades, Section 1, Dorchester Rapid Transit.

IRWIN J. HENNESSY. Plans and details for steelwork, Dorchester Rapid Transit.

JOHN F. HOWARD. Plans and details for Dorchester Rapid Transit.

RALPH A. PLUNKETT. Plans and details for Dorchester Rapid Transit.

KARL R. SAUNDERS. Plans and details for Dorchester Rapid Transit.

Transitmen.

ROBERT S. BOWES. Lines and grades, Sections 2 and 3, Dorchester Rapid Transit.

PATRICK H. BOYLE. Lines and grades, Sections 1 and 4, Dorchester Rapid Transit.

THOMAS E. CARNEY. Lines and grades, Sections 2 and 4, Dorchester Rapid Transit.

MICHAEL J. DRISCOLL. Lines and grades, Sections 2 and 3, Dorchester Rapid Transit.

MICHAEL S. FOGARTY. Quantities for estimates, Sections 2 and 4, Dorchester Rapid Transit.

GEORGE G. HYLAND. Lines and grades, Sections 1, 2 and 4, Dorchester Rapid Transit.

ALBERT I. McDERMOTT. Plans and details for Dorchester Rapid Transit.

JOSEPH J. O'BRIEN. Lines and grades, Section 4, Dorchester Rapid Transit.

EDWARD G. A. POWERS. Lines and grades, Sections 3 and 4, Dorchester Rapid Transit.

THOMAS E. RAFFERTY. Quantities for estimates, Section 3, Dorchester Rapid Transit.

FRANK A. RULL. Photography and blueprinting.

JOHN J. RYAN. Lines and grades, Sections 1 and 4, Dorchester Rapid Transit.

FREDERIC W. STILES. Care of plans, survey records, etc., general office work.

THOMAS D. SULLIVAN. Lines and grades, Section 1, Dorchester Rapid Transit.

Rodmen.

WALTER H. BARRETT. Lines and grades, Sections 1, 2, 3 and 5, Dorchester Rapid Transit.

JOHN W. BURKE. Lines and grades, Sections 3 and 4, Dorchester Rapid Transit.

* PAUL J. CANTY. Lines and grades, Section 3, Dorchester Rapid Transit.

JOHN F. COUGHLIN. Lines and grades, Sections 2 and 3, Dorchester Rapid Transit.

JOHN P. CROTTY, JR. Plans and tracings, Dorchester Rapid Transit.

STEPHEN C. DESROCHE. Lines and grades, Sections 2 and 4, Dorchester Rapid Transit.

* CHARLES M. FITZGERALD. Lines and grades, Section 3, Dorchester Rapid Transit.

JAMES J. FITZGERALD. Lines and grades, Sections 3 and 4, Dorchester Rapid Transit.

JOHN P. GLYNN. Lines and grades, Sections 1 and 4, Dorchester Rapid Transit.

JOSEPH F. HIGGINS. Lines and grades, Sections 2 and 3, Dorchester Rapid Transit.

* S. FRANCIS LITTLETON, JR. Lines and grades, Section 4, Dorchester Rapid Transit.

FREDERICK H. MARLEY. Lines and grades, Sections 3 and 4, Dorchester Rapid Transit.

WILLIAM McLAUGHLIN. Lines and grades, Sections 2 and 4, Dorchester Rapid Transit.

* CHARLES D. NORTON. Lines and grades, Sections 1 and 4, Dorchester Rapid Transit.

* GEORGE P. NORTON. Lines and grades, Section 1, Dorchester Rapid Transit.

* KENNETH T. NORTON. Lines and grades, Section 3, Dorchester Rapid Transit.

* Left the employ of the Transit Department.

- * PATRICK O'CONNOR. Lines and grades, Section 2, Dorchester Rapid Transit.
- JAMES A. O'ROURKE. Lines and grades, Sections 1, 3 and 4, Dorchester Rapid Transit.
- RAYMOND V. PAGE. Lines and grades, Section 4, Dorchester Rapid Transit.
- * ANTHONY J. PERRY. Lines and grades, Section 2, Dorchester Rapid Transit.
- JOHN A. ROONEY. Lines and grades, Sections 1, 3 and 4, Dorchester Rapid Transit.
- * JOHN C. ROONEY. Lines and grades, Section 3, Dorchester Rapid Transit.
- HERBERT J. STONE. Lines and grades, Sections 3 and 4, Dorchester Rapid Transit.
- * JAMES F. SULLIVAN. Lines and grades, Section 3, Dorchester Rapid Transit.
- LEO J. SULLIVAN. Lines and grades, Sections 1 and 5, Dorchester Rapid Transit.

Inspectors and Others.

- THOMAS H. ARMSTRONG. Inspection, Sections 2 and 4, Dorchester Rapid Transit.
- HAROLD M. BRADBURY. Inspection, Sections 2 and 3, Dorchester Rapid Transit.
- JOHN DOHERTY. Inspection, Sections 3 and 4, Dorchester Rapid Transit.
- JAMES F. DRISCOLL. Inspection, Sections 2 and 4, Dorchester Rapid Transit.
- WILLIAM J. DRUMMOND. Inspection, Sections 1, 3 and 4, Dorchester Rapid Transit.
- JOHN J. FALLON, JR. Inspection, Sections 2 and 3, Dorchester Rapid Transit.
- JOHN L. GEOGHEGAN. Inspection, Sections 1 and 4, Dorchester Rapid Transit.
- JOSEPH J. JOLLEY. Inspection, Sections 3 and 4, Dorchester Rapid Transit.
- AUSTIN E. JOYCE. Inspection, Section 3, Dorchester Rapid Transit.
Grouting, Boylston Street Subway, Dorchester Tunnel and East Boston Tunnel Extension.
- THOMAS H. KEENAN. Inspection, Section 3, Dorchester Rapid Transit.
- STEPHEN V. McHALE. Inspection, Sections 2 and 4, Dorchester Rapid Transit.
- EMANUEL N. REINHALTER. Inspection, Dorchester Rapid Transit.
- JOSEPH E. REINHALTER. Inspection, Section 1, Dorchester Rapid Transit
and at alterations of Scollay Square Entrance and Exit Building.
- GEORGE E. SULLIVAN. Construction Accounts.
- JAMES E. WARD. Inspection, Sections 1 and 4, Dorchester Rapid Transit.

Clerical Force.

- JOHN J. BRADLEY. Clerk — Timekeeping and daily reports, Sections 1 and 4, Dorchester Rapid Transit.
- KATHERINE I. DRISCOLL. Clerk and Stenographer.

* Left the employ of the Transit Department.

JOHN J. FARREN. Construction Cost Accountant.

PAUL J. KELLAHER. Clerk and Stenographer — Timekeeping and daily reports, Sections 2, 3 and 4, Dorchester Rapid Transit.

MARY E. McKERNAN. Clerk.

MABEL A. MURPHY. Stenographer.

THOMAS J. MULDOON. Clerk and Stenographer at Cypher Street Stockyard.

HENRY F. HORADAN. Blueprinting.

WILLIAM J. SKIFFINGTON. Blueprinting, Photography, etc.

RICHARD F. TOBIN. Field Work, Sections 1 and 5, Dorchester Rapid Transit.

APPENDIX III.

BIDS FOR FURNISHING AND DELIVERING ABOUT 125,400 FEET OF BRAID OR LEAD COVERED INSULATED CABLE AND WIRE. JANUARY 11, 1927.

BIDDER.	Estimated Price.	Time.
Standard Underground Cable Company *	\$19,750.53	6-8 weeks
Hazard Manufacturing Company †.....	19,221.636	65 days
American Steel & Wire Company †.....	19,118.21	8-9 weeks
Simplex Wire & Cable Company †.....	18,818.71	8-10 "
Bishop Wire & Cable Corporation †.....	17,899.328	60-90 days

* Awarded. Contract 824.

† Omitted 1 Item.

APPENDIX IV.

BIDS FOR FURNISHING AND INSTALLING A SYSTEM OF PLUMBING AT COLUMBIA STATION, DORCHESTER RAPID TRANSIT. JANUARY 25, 1927.

BIDDER.	Estimated Price.	Time.
P. F. Russo Company, Inc.....	\$2,469.00	30 days
William H. Mitchell & Son Company..	2,078.00	25 "
Low & Robins Company.....	2,062.00	40 "
David Craig.....	1,980.00	35 "
William N. McKenna Company.....	1,945.00	rapidly as bldg. progresses
Capital Plumbing & Heating Company,	1,920.00	28 days
Garvey Plumbing & Heating Company,	1,800.00	30 "
Edward C. Kelly *.....	1,707.00	60 "

* Awarded. Contract 827.

APPENDIX V.

BIDS FOR FURNISHING AND DELIVERING CERTAIN SPECIAL TRACK WORK FOR THE DORCHESTER RAPID TRANSIT. JANUARY 25, 1927.

BIDDER.	Estimated Price.
Lorain Steel Company.....	\$55,080.00
Bethlehem Steel Company.....	54,635.00
William Wharton, Jr. & Company, Inc.**.....	53,163.00
Barbour Stockwell Company *.....	28,738.00

* Awarded. Contract 828. 2 Items.

** Awarded. Contract 829. 5 Items.

APPENDIX VI.

BIDS FOR FURNISHING AND DELIVERING SPECIAL WOOD SHAPES AND
WOODEN TRUNKING. FEBRUARY 1, 1927.

BIDDER.	Estimated Price.
W. O. McDuffee Company.....	\$6,831.20
Graham & Cameron, Inc.*.....	5,225.80
L. A. Sprague.....	1,653.02
H. A. Stiles & Company **.....	1,229.17

3 Irregular Bids. All Rejected.

* Awarded. Contract 832. 6 Items.

** Awarded. Contract 833. 1 Item.

APPENDIX VII.

BIDS FOR FURNISHING AND DELIVERING 5,000 TONS OF CRUSHED STONE
FOR TRACK BALLAST, DELIVERED ALONG THE DORCHESTER RAPID
TRANSIT RIGHT OF WAY, BETWEEN DORCHESTER AVENUE, SOUTH
BOSTON AND CRESCENT AVENUE, DORCHESTER. FEBRUARY 23, 1927.

BIDDER.	Estimated Price.
West Roxbury Trap Rock Company.....	\$2.50 per ton
New England Crushed Stone Company.....	2.50 "
Old Colony Crushed Stone Company.....	2.25 "
William J. Barry *.....	2.25 "

* Awarded. Contract 835.

APPENDIX VIII.

BIDS FOR FURNISHING AND DELIVERING 5,000 TONS OF CRUSHED STONE
FOR TRACK BALLAST, DORCHESTER RAPID TRANSIT, DELIVERED AT
BOSTON ELEVATED RAILWAY YARD, GEORGE STREET, EAST SOMER-
VILLE, MASS. FEBRUARY 23, 1927.

BIDDER.	Estimated Price.
William J. Barry.....	\$2.00 per ton
Rowe Contracting Company *.....	1.85 " by truck

* Awarded. Contract 836.

APPENDIX IX.

BIDS FOR BUILDING SAVIN HILL STATION ENTRANCE-EXIT BUILDING AND
ADJUNCTS, SECTION ONE, DORCHESTER RAPID TRANSIT. MARCH 22,
1927.

BIDDER.	Estimated Price.	Time.
J. P. Foley & Sons.....	\$46,164.00	135 days
H. L. Hauser Building Company, Inc.....	44,285.00	90 "
M. F. Gaddis.....	42,735.00	100 "
Guiney & Hanson Construction Company..	41,650.00	135 "
Luke S. White, Inc.....	40,000.00	90 "
A. Piotti Company.....	38,949.00	120 "
John B. Dolan.....	38,070.00	120 "
J. Slotnik Company.....	37,989.00	95 "
J. A. Singarella *.....	37,500.00	90 "

* Awarded. Contract 840.

APPENDIX X.

BIDS FOR CERTAIN SPECIAL TRACKWORK FOR CODMAN YARD,
DORCHESTER RAPID TRANSIT. APRIL 5, 1927.

BIDDER.	Estimated Price.
Bethlehem Steel Company.....	\$51,180.00
Barbour Stockwell Company.....	49,564.00
William Wharton Jr. & Company, Inc.....	47,621.00
Lorain Steel Company *.....	43,842.00

* Awarded. Contract 843.

APPENDIX XI.

BIDS FOR FURNISHING AND ERECTING 5,500 LINEAR FEET, MORE OR LESS,
OF WOVEN WIRE FENCE ALONG THE RIGHT OF WAY OF THE DOR-
CHESTER RAPID TRANSIT. APRIL 27, 1927.

BIDDER.	Estimated Price.	Time.
C. A. Gates & Company.....	\$8,091.25	45 days
P. J. Dinn & Company.....	8,085.75	40 "
Security Fence Company.....	8,085.75	60 "
W. A. Snow Iron Works Company.....	8,085.75	45 "
American Chain Link Fence Company *...	7,695.00	90 "

* Awarded. Contract 844.

APPENDIX XII.

BIDS FOR FURNISHING AND DELIVERING 13,000 TONS, MORE OR LESS, OF CRUSHED STONE FOR TRACK BALLAST ALONG THE DORCHESTER RAPID TRANSIT RIGHT OF WAY BETWEEN COLUMBIA ROAD AND GENEVA AVENUE, DORCHESTER. MAY 17, 1927.

BIDDER.	Estimated Price.
General Crushed Stone Company	\$2.65 per ton
West Roxbury Trap Rock Company	2.50 "
William J. Barry	2.45 "
Old Colony Crushed Stone Company *	2.12½ "

* Awarded. Contract 846.

APPENDIX XIII.

BIDS FOR FURNISHING AND DELIVERING, TRANSPORTATION PREPAID, AT GENEVA AVENUE AND GREELEY STREET, NEAR FIELDS CORNER, DORCHESTER, MASS., 113,000 FEET BOARD MEASURE OF LONG-LEAF YELLOW PINE LUMBER. MAY 17, 1927.

BIDDER.	Estimated Price.	Delivery.
Pope Lumber Company	\$7,192.54	None
Johnson Battle Lumber Company	4,947.80	45 days
The Prendergast Company	4,835.35	45 "
George McQuesten Company *	4,441.78	45 "

* Awarded. Contract 847.

APPENDIX XIV.

BIDS FOR FURNISHING AND ERECTING 5,000 LINEAR FEET, MORE OR LESS, OF WOVEN WIRE FENCE AND 2,000 LINEAR FEET, MORE OR LESS, OF CONCRETE FENCE POST FOUNDATIONS AND CURBS ALONG THE RIGHT OF WAY OF THE DORCHESTER RAPID TRANSIT. MAY 25, 1927.

BIDDER.	Estimated Price.	Time.
American Fence Construction Company...	\$15,150.52	90 days
A. G. Tomasello & Son, Inc.....	14,460.00	60 "
P. J. Dinn & Company.....	14,457.00	90 "
Walter E. MacEachern.....	13,796.00	65 "
Security Fence Company.....	12,597.00	None
W. A. Snow Iron Works, Inc.....	12,141.00	90 days
C. A. Gates Company *.....	11,235.00	60 "

* Awarded. Contract 848.

APPENDIX XV.

BIDS FOR LAYING ABOUT 500 SQUARE YARDS OF SHEET ASPHALT OR BITULITHIC PAVEMENT AND ARTIFICIAL STONE SIDEWALKS IN COLUMBIA ROAD NEAR THE BRIDGE OVER DORCHESTER RAPID TRANSIT TRACKS, SECTION ONE. JUNE 1, 1927.

BIDDER.	Estimated Price.	Time.
<i>Asphalt.</i>		
A. G. Tomasello & Son, Inc.....	\$3,046.50	30 days
J. A. Singarella.....	2,472.75	30 "
Samuel J. Tomasello *.....	2,137.50	30 "
<i>Bitulithic.</i>		
A. G. Tomasello & Son, Inc.....	3,046.50	30 "
J. A. Singarella.....	2,472.75	30 "
Samuel J. Tomasello.....	2,208.75	30 "

* Awarded. Contract 849.

APPENDIX XVI.

BIDS FOR FURNISHING AND ERECTING SECTIONS B, C AND E OF WOVEN WIRE FENCE WITH CONCRETE FENCE POST FOUNDATIONS AND CURBS ALONG THE RIGHT OF WAY OF THE DORCHESTER RAPID TRANSIT. JUNE 8, 1927.

BIDDER.	Estimated Price.	Time.
<i>Section B.</i>		
A. G. Tomasello & Son, Inc.....	\$22,131.25	Not Stated
P. J. Dinn & Company.....	20,428.50	120 days
Security Fence Company.....	19,444.00	60 "
C. A. Gates & Company.....	18,520.00	70 "
J. A. Singarella *.....	18,187.75	90 "
<i>Section C.</i>		
A. G. Tomasello & Son, Inc.....	18,715.00	Not Stated
P. J. Dinn & Company.....	18,019.00	120 days
Security Fence Company.....	16,691.00	75 "
C. A. Gates & Company.....	16,677.00	70 "
J. A. Singarella *.....	16,076.00	90 "
<i>Section E.</i>		
P. J. Dinn & Company.....	10,910.25	120 "
C. A. Gates & Company.....	10,734.00	60 "
Security Fence Company.....	10,654.80	90 "
J. A. Singarella.....	9,513.20	90 "
A. G. Tomasello & Son, Inc.**.....	9,153.00	Not Stated

* Awarded. Contract 850.

** Awarded. Contract 851.

APPENDIX XVII.

BIDS FOR FURNISHING AND DELIVERING, F. O. B. CARS, 14 CYPHER STREET, SOUTH BOSTON, ABOUT 330 TONS OF STRUCTURAL STEEL, CONSISTING OF BEAMS, CHANNELS AND ROLLED STEEL SLABS. JUNE 15, 1927.

BIDDER.	Estimated Price.
Bethlehem Steel Company *.....	\$15,554.85

* Awarded. Contract 852.

APPENDIX XVIII.

BIDS FOR FURNISHING AND INSTALLING ROOFING AND COPPER WORK,
FIELDS CORNER STATION. JUNE 22, 1927.

BIDDER.	Estimated Price.	Time.
E. Van Noorden Company.....	\$8,572.00	30 days
T. C. Brooks Company.....	8,300.00	22 "
W. A. Murtfeldt Company.....	8,223.00	22 "
Highland Sheet Metal Works.....	8,076.00	50 "
John C. Finegan Company.....	8,000.00	15 "
Cavanaugh & Earley, Inc.....	7,975.00	30 "
Madden & Sons, Inc.....	7,379.00	15 "
Columbia Cornice Company.....	7,025.00	35 "
T. J. Flynn Metal Works *.....	6,990.00	35 "

* Awarded. Contract 853.

APPENDIX XIX.

BIDS FOR FURNISHING CENTRIFUGAL PUMPS FOR DORCHESTER RAPID
TRANSIT. JUNE 22, 1927.

BIDDER.	Estimated Price.	Time.
Starkweather & Broadhurst, Inc.*.....	\$1,275.00	56 days
Hayes Pump & Machinery Company †....	1,272.00	90 "

* Awarded. Contract 854.

† Bid did not comply with terms of specifications.

APPENDIX XX.

BIDS FOR DOING CARPENTER WORK, PAINTING, ETC., FIELDS CORNER
STATION. JULY 6, 1927.

BIDDER.	Estimated Price.	Time.
Beaver Construction Company.....	\$33,500.00	150 days
M. S. Keliher Company.....	28,297.00	92 "
J. A. Singarella *.....	24,500.00	90 "

* Awarded. Contract 855.

APPENDIX XXI.

BIDS FOR FURNISHING AND INSTALLING A SYSTEM OF PLUMBING, SAVIN HILL STATION, DORCHESTER RAPID TRANSIT, JULY 6, 1927.

BIDDER.	Estimated Price.	Time.
William J. Donohue.....	\$2,785.00	35 days
Henry J. Lyons Company.....	2,390.00	35 "
William H. Mitchell & Son Company.....	2,225.00	20 "
The Downey Company.....	2,132.00	8 weeks
Garvey Plumbing & Heating Company....	2,078.00	30 days
P. F. Russo Company, Inc.....	1,987.00	30 "
Edward C. Kelly *.....	1,889.00	30 "

* Awarded. Contract 856.

APPENDIX XXII.

BIDS FOR FURNISHING 33,600 LINEAR FEET, MORE OR LESS, OF VITRIFIED TERRA COTTA DUCTS FOR DORCHESTER RAPID TRANSIT, JULY 13, 1927.

BIDDER.	Estimated Price per Linear Foot.	Delivery.
R. L. Winslow Company, Inc.....	9.25 cents	30 days
Shawmut Clay Manufacturing Company..	9.25 cents	10 "
National Fireproofing Company*.....	8.3 cents	14 "

* Awarded. Contract 857.

APPENDIX XXIII.

BIDS FOR PLASTERING WALLS AND CEILINGS AT FIELDS CORNER STATION, DORCHESTER RAPID TRANSIT. JULY 20, 1927.

BIDDER.	Estimated Price.	Time.
A. G. Tomasello & Son, Inc.....	\$11,550.00	40 days
Muir Brothers Company.....	10,979.25	45 "
A. V. Day & Company.....	4,908.75	30 "
George A. Craffey Company.....	4,705.00	15 "
Stephen T. Keith Company *.....	4,654.25	40 "

* Awarded. Contract 858.

APPENDIX XXIV.

BIDS FOR FURNISHING AND DELIVERING, FREIGHT PREPAID, F. O. B. CARS, 14 CYPHER STREET, SOUTH BOSTON, ABOUT 400 TONS OF STRUCTURAL STEEL, CONSISTING OF BEAMS, CHANNELS AND ROLLED STEEL SLABS. JULY 27, 1927.

BIDDER.	Estimated Price.	Delivery.
Bethlehem Steel Company *.....	\$20,948.71	35 days

* Awarded. Contract 859.

APPENDIX XXV.

BIDS FOR FURNISHING AND INSTALLING A SYSTEM OF PLUMBING AT FIELDS CORNER STATION, DORCHESTER RAPID TRANSIT. JULY 27, 1927.

BIDDER.	Estimated Price.	Time.
Garvey Plumbing & Heating Company....	\$7,736.00	60 days
Edward C. Kelly.....	7,711.00	35 "
William H. Mitchell & Son Company.....	7,690.00	35 "
The Downey Company.....	7,210.00	60 "
Walsh & Brown Company *.....	6,947.00	60 "

* Awarded. Contract 860.

APPENDIX XXVI.

BIDS FOR FURNISHING AND INSTALLING HAND RAILS AND HAND-RAIL FITTINGS AT FIELDS CORNER STATION, DORCHESTER RAPID TRANSIT. AUGUST 3, 1927.

BIDDER.	Estimated Price.	Time.
Frank H. Letteney.....	\$2,063.33	30 days
James A. Glass *.....	1,238.90	30 "

* Awarded. Contract 861.

APPENDIX XXVII.

BIDS FOR FURNISHING CERTAIN ELECTRICAL EQUIPMENT FOR ASHMONT
SUBSTATION, DORCHESTER RAPID TRANSIT, AUGUST 3, 1927.

BIDDER.	Estimated Price.	Delivery.
Westinghouse Electric & Manufacturing Company.....	\$80,985.00	210 days
General Electric Company *.....	74,400.00	210 "

* Awarded. Contract 862.

APPENDIX XXVIII.

BIDS FOR FURNISHING AND DELIVERING, FREIGHT PREPAID, F. O. B.
CARS, WALWORTH SIDING, SOUTH BOSTON, CERTAIN SPECIAL TRACK-
WORK FOR THE DORCHESTER RAPID TRANSIT, AUGUST 17, 1927.

BIDDER.	Estimated Price.	Delivery.
Lorain Steel Company.....	\$6,242.00	63 days
William Wharton, Jr. & Company.....	5,713.00	10-12 weeks
Barbour Stockwell Company *.....	5,507.00	5- 7 "

* Awarded. Contract 863.

APPENDIX XXIX.

BIDS FOR FURNISHING AND ERECTING WOVEN WIRE FENCE WITH CON-
CRETE FENCE POST FOUNDATIONS AND CURBS ALONG THE RIGHT OF
WAY AND ELSEWHERE NEAR THE FIELDS CORNER STATION OF THE
DORCHESTER RAPID TRANSIT, SEPTEMBER 8, 1927.

BIDDER.	Estimated Price.	Time.
A. G. Tomasello & Son, Inc.....	\$5,465.00	30 days
Beaver Construction Company.....	5,415.45	35 "
P. J. Dinn & Company.....	5,340.27	45 "
Security Fence Company.....	4,998.00	45 "
J. A. Singarella*.....	4,133.50	20 "

* Awarded. Contract 867.

APPENDIX XXX.

BIDS FOR DEMOLISHING AND REMOVING THE MAIN ENTRANCE AND EXIT BUILDING, SCOLLAY SQUARE STATION, SEPTEMBER 14, 1927.

BIDDER.	Estimated Price.	Time.
New England Building Wrecking Company,	\$8,995.00	20 days
City Building Wrecking Company.....	2,970.00	12 "
American Building Wrecking Company....	2,875.00	12 "
Central Building Wrecking Company*....	2,395.00	20 "

* Awarded. Contract 868.

APPENDIX XXXI.

BIDS FOR FURNISHING AND ERECTING PARAPET WALLS OF POLISHED DARK QUINCY GRANITE AT THE MAIN SUBWAY ENTRANCE IN SCOLLAY SQUARE, SEPTEMBER 14, 1927.

BIDDER.	Estimated Price.	Time.
J. A. Singarella.....	\$5,240.00	60 days
Deacon Brothers, Inc.*.....	1,915.00	90 "

* Awarded. Contract 869.

Contract given up by Deacon Bros. claiming error in figures.

APPENDIX XXXII.

BIDS FOR FURNISHING AND DELIVERING, FREIGHT PREPAID, F. O. B. CARS, 14 CYPHER STREET, SOUTH BOSTON, 25,000 LINEAR FEET, MORE OR LESS, OF FIBRE CONDUIT, 3½-INCH INSIDE DIAMETER, 1,000, MORE OR LESS, OF EXTRA COUPLINGS FOR 3½-INCH CONDUIT AND 375, 3½-INCH 90 DEGREES BENDS, SEPTEMBER 14, 1927.

BIDDER.	Estimated Price.	Delivery.
Graybar Electric Company.....	\$3,314.80	15 days
Johns-Manville, Inc.*.....	3,314.80	10 "

* Awarded. Contract 870.

APPENDIX XXXIII.

CANVASS OF BIDS FOR SECTION 4, DORCHESTER RAPID TRANSIT, AUGUST 30, 1927

	a	2a	3a	b	c	2c	3c	4c	5c	6c	7c	8c	d	e	2e	3e	f	2f	3f	g	2g	3g	h	k	l	m	n	2n	3n	4n	5n	6n	7a	p	2p	3p	r	Totals	Time of Completion.	
BIDDERS AND ADDRESSES	Earth Excavation 20,000 Cu. Yds.	Rock Excavation 500 Cu. Yds.	Gravel Borrow 35,000 Cu. Yds.	Masonry Retaining 1,500 Cu. Yds.	Concrete (Rein.) 1,000 Cu. Yds.	Concrete (Plain) 4,000 Cu. Yds.	Small Stone 1 concrete 200 Cu. Yds.	Cinder Concrete 1,000 Cu. Yds.	Mortar 150 Cu. Yds.	Concrete Finish 4,000 Sq. Yds.	Gravel Finish 100 Sq. Yds.	Gravel Finish Walls 500 Sq. Yds.	Brick Masonry 150 Cu. Yds.	Verified Pipe 4"-6"-8" 1,000 Lin. Ft.	Verified Pipe 10"-12" 1,000 Lin. Ft.	Verified Pipe 15" to 24" 500 Lin. Ft.	Reinforcing Rods, Etc., 450 Tons.	Structural Steel 850 Tons.	Wire Cloth 25,000 Sq. Ft.	Plaster 100 Sq. Yds.	Grout 1-1 50 Cu. Yds.	Grout 1-4 50 Cu. Yds.	Water-proofing 7,500 Sq. Yds.	Incidental Work Kuka, Supporting Structures, Etc.	Renovating Peabody Bldg. Bridge Timber & Flooring	Wall Finish Building 10,000 Sq. Yds.	Conduit Lines 1 to 12 100 ft. 31,000 Lin. Ft.	Power Mainlines Type I 8 M. H.	Power Mainlines Type II 2 M. H.	Signal Mainlines with Signal Foundations Type III 5 M. H.	Signal Foundations Type IV 10 F'd'n.	Signal Foundations Type V 3 F'd'n.	Signal A. Power Mainlines Type VI 3 M. H.	Gravel Retaining 100 Sq. Yds.	Gravel Retaining 100 Lin. Ft.	Excavating 200 Lin. Ft.	Preparing Site			
J. C. Coleman & Sons Co., 1620 Tremont St., Boston.	\$2.50 60,000.00	\$2.50 1,250.00	\$2.00 70,000.00	\$10.00 15,000.00	\$20.00 140,000.00	\$16.00 16,000.00	\$20.00 4,000.00	\$18.00 15,000.00	\$20.00 3,000.00	\$1.75 3,000.00	\$2.00 200.00	\$3.00 1,500.00	\$55.00 7,500.00	\$1.00 3,000.00	\$2.00 2,000.00	\$3.00 100.00	\$60.00 30,000.00	\$30.00 25,000.00	\$0.15 3,750.00	\$1.00 100.00	\$25.00 1,250.00	\$20.00 1,000.00	\$0.75 5,625.00	\$25,000.00 25,000.00	\$5,000.00 5,000.00	\$1.00 10,000.00	\$1.00 1,000.00	\$200.00 1,000.00	\$300.00 1,500.00	\$300.00 1,500.00	\$250.00 1,250.00	\$200.00 1,000.00	\$250.00 1,250.00	\$200.00 1,000.00	\$1.50 150.00	\$2.50 250.00	\$0.75 150.00	\$10,000.00 10,000.00	\$557,475.00	300 Days
John McCourt Co., 930 Huntington Ave., Boston.	1.65 33,000.00	6.00 3,000.00	1.65 57,750.00	5.00 7,500.00	25.00 207,000.00	23.00 23,000.00	25.00 5,000.00	12.00 12,000.00	25.00 1,200.00	0.25 1,000.00	1.50 150.00	4.00 2,000.00	10.00 6,000.00	1.00 3,000.00	2.00 2,000.00	3.00 100.00	100.00 15,000.00	30.00 31,000.00	0.15 3,750.00	1.00 100.00	20.00 1,000.00	10.00 500.00	1.00 7,500.00	10,000.00 10,000.00	10,000.00 10,000.00	0.50 5,000.00	0.40 12,000.00	250.00 2,000.00	250.00 2,000.00	250.00 1,250.00	250.00 1,000.00	100.00 1,000.00	50.00 500.00	0.00 0.00	1.00 100.00	0.00 0.00	120.00 12,000.00	500,370.00	350 Days	
James H. Fadden, 20 Powder House Terrace, Somerville.	2.50 60,000.00	5.00 2,500.00	1.75 61,250.00	5.00 12,000.00	15.00 102,000.00	15.00 15,000.00	21.00 4,200.00	12.00 12,000.00	20.00 3,000.00	0.40 1,000.00	1.00 100.00	3.00 1,500.00	10.00 6,000.00	1.00 2,000.00	1.55 1,550.00	1.00 100.00	100.00 40,000.00	30.00 25,000.00	0.10 4,500.00	1.00 100.00	25.00 1,250.00	10.00 500.00	0.75 5,000.00	5,000.00 5,000.00	1,000.00 1,000.00	0.50 5,000.00	0.50 15,000.00	200.00 1,000.00	225.00 1,125.00	250.00 1,250.00	250.00 1,250.00	75.00 750.00	75.00 750.00	2.00 200.00	0.75 5,000.00	5.00 5,000.00	100,000.00 100,000.00	677,450.00	100 Days	
A. G. Tomassello & Son, Inc., 250 Stuart St., Boston.	1.10 22,000.00	5.00 2,500.00	1.00 35,000.00	5.00 7,500.00	20.00 140,000.00	15.00 15,000.00	20.00 4,000.00	10.00 10,000.00	30.00 1,500.00	0.50 2,000.00	1.00 100.00	3.00 1,500.00	10.00 6,000.00	1.00 2,000.00	1.10 1,100.00	2.00 100.00	100.00 45,000.00	25.00 21,250.00	0.10 2,500.00	2.00 200.00	25.00 1,250.00	10.00 500.00	0.75 5,000.00	2,000.00 2,000.00	2,000.00 2,000.00	1.00 10,000.00	0.50 15,000.00	200.00 1,000.00	200.00 1,000.00	200.00 1,000.00	50.00 500.00	100.00 1,000.00	100.00 1,000.00	1.00 100.00	1.00 100.00	1.00 100.00	0.00 0.00	100,000.00 100,000.00	144,025.00	345 Days
M. F. Gaglia, 245 State St., Boston.	1.25 25,000.00	7.00 3,500.00	1.20 42,000.00	7.00 10,500.00	22.00 175,000.00	21.00 21,000.00	22.00 4,000.00	5.00 5,000.00	3.00 3,000.00	0.25 1,000.00	1.00 100.00	3.00 1,500.00	10.00 6,000.00	1.00 2,000.00	1.50 1,500.00	1.00 100.00	100.00 45,000.00	15.00 32,550.00	0.12 3,000.00	0.50 500.00	10.00 500.00	10.00 500.00	0.50 3,750.00	100.00 1,000.00	1,000.00 1,000.00	0.50 5,000.00	0.30 1,000.00	200.00 1,000.00	200.00 1,000.00	250.00 1,250.00	250.00 1,250.00	250.00 1,250.00	50.00 500.00	50.00 500.00	1.00 100.00	0.50 500.00	5.00 5,000.00	100,000.00 100,000.00	144,000.00	355 Days
Coleman Bros. Inc., 245 State St., Boston.	2.10 40,000.00	4.00 2,000.00	1.00 56,000.00	10.00 21,000.00	30.00 171,000.00	32.00 13,000.00	27.00 5,000.00	7.00 7,000.00	20.00 3,000.00	0.40 1,000.00	1.00 100.00	2.75 1,375.00	10.00 6,000.00	0.75 2,250.00	1.00 1,000.00	2.00 100.00	100.00 35,250.00	30.00 26,500.00	0.00 2,250.00	1.00 100.00	20.00 1,000.00	15.00 750.00	0.50 3,000.00	2,000.00 2,000.00	2,000.00 2,000.00	0.50 5,000.00	0.50 15,000.00	200.00 1,000.00	200.00 1,000.00	250.00 1,250.00	250.00 1,250.00	250.00 1,250.00	50.00 500.00	50.00 500.00	1.00 100.00	0.50 500.00	5.00 5,000.00	100,000.00 100,000.00	143,730.00	390 Days
Joseph A. Sugarbaker, 110 High St., Boston.	1.60 32,000.00	7.00 3,500.00	1.40 40,000.00	5.00 7,500.00	15.00 105,000.00	15.00 15,000.00	20.00 4,000.00	0.00 0.00	30.00 4,500.00	0.45 1,800.00	3.00 300.00	4.50 2,225.00	10.00 4,500.00	0.75 2,250.00	1.00 1,000.00	3.00 300.00	100.00 35,250.00	30.00 25,500.00	0.10 2,500.00	2.00 200.00	20.00 1,000.00	10.00 500.00	1.00 7,500.00	10,000.00 10,000.00	5,000.00 5,000.00	0.50 5,000.00	0.40 12,000.00	200.00 1,000.00	200.00 1,000.00	250.00 1,250.00	250.00 1,250.00	250.00 1,250.00	50.00 500.00	50.00 500.00	1.00 100.00	0.75 5,000.00	5.00 5,000.00	100,000.00 100,000.00	429,825.00	350 Days
C. & R. Construction Co., 75 Bradlee St., Roxbury.	0.80 16,000.00	0.00 0.00	0.80 28,000.00	0.00 1,000.00	20.50 143,500.00	20.50 20,500.00	30.00 6,000.00	5.00 5,000.00	10.00 4,500.00	0.10 400.00	2.00 200.00	3.00 1,500.00	10.00 1,500.00	0.80 2,400.00	1.00 1,000.00	3.00 300.00	100.00 37,250.00	25.00 21,250.00	0.10 2,500.00	0.00 0.00	5.00 250.00	5.00 250.00	0.00 4,500.00	15,000.00 15,000.00	11,000.00 11,000.00	0.00 4,000.00	0.25 7,500.00	200.00 1,000.00	300.00 1,500.00	250.00 1,250.00	100.00 500.00	100.00 500.00	25.00 250.00	5.00 500.00	2.00 200.00	0.50 500.00	17,000.00 17,000.00	401,565.00	300 Days	

Contract awarded to C & R Construction Co., September 14, 1927. Contract #72

APPENDIX XXXIV.

BIDS FOR BUILDING SHAWMUT STATION ENTRANCE-EXIT BUILDING,
SECTION THREE, DORCHESTER RAPID TRANSIT, SEPTEMBER 14, 1927.

BIDDER.	Estimated Price.	Time.
Lawrence Tocci.....	\$33,300.00	180 days
M. S. Kelliher Company.....	28,000.00	115 "
I. F. Woodbury & Sons Company.....	26,509.00	125 "
Joseph A. Singarella.....	25,440.00	75 "
A. Bartington.....	25,300.00	75 "
Thomas O'Connor & Company.....	24,000.00	90 "
John M. Tobin, Inc.....	23,740.00	77 "
John B. Dolan.....	23,695.00	90 "
A. Piotti Company.....	23,567.00	70 "
J. Slotnik Company.....	23,488.00	100 "
White Construction Company, Inc.*.....	23,434.00	72 "
P. J. Cantwell †.....	23,290.00	90 "

* Awarded. Contract 871.

† Did not conform to requirements of proposal.

APPENDIX XXXV.

BIDS FOR FURNISHING AND ERECTING PARAPET WALLS OF POLISHED
DARK QUINCY OR ROCKPORT GRANITE AT THE MAIN SUBWAY EN-
TRANCE IN SCOLLAY SQUARE, OCTOBER 6, 1927.

BIDDER.	Estimated Price.	Time.
J. A. Singarella.....	\$4,300.00	60 days
A. Bartington.....	2,995.00	60 "
Rockport Granite Company.....	2,950.00	36 "
Banspar Construction Company.....	2,770.00	35 "
Forbes, Craig & Company.....	2,750.00	8 weeks
M. Solimando *.....	2,160.00	45 days

* Awarded. Contract 874.

APPENDIX XXXVI.

BIDS FOR FURNISHING AND DELIVERING, FREIGHT PREPAID, F. O. B. CARS, WALWORTH SIDING, SOUTH BOSTON, CERTAIN SPECIAL TRACK—WORK FOR THE DORCHESTER RAPID TRANSIT, OCTOBER 19, 1927.

BIDDER.	Estimated Price.	Delivery.
Bethlehem Steel Company.....	\$29,758.00	96 days
Barbour Stockwell Company **.....	28,072.00	not later than 2/1/28
Lorain Steel Company.....	27,973.00	96 days
William Wharton, Jr. & Company, Inc.*...	27,300.00	{ a — 13 wks b — 9 " c — 13 "

* Awarded. Contract 877. 2 items.

** Awarded. Contract 878. 1 item.

APPENDIX XXXVII.

BIDS FOR BUILDING A 12-INCH PIPE SEWER AND APPURTENANCES IN HUBBARD ROAD, DORCHESTER RAPID TRANSIT, SECTION ONE, NOVEMBER 3, 1927.

BIDDER.	Estimated Price.	Time.
J. A. Singarella.....	\$5,770.00	30 days
Charles Struzziery.....	4,910.00	35 "
M. DeSisto Company.....	4,535.605	60 "
M. Solimando.....	4,405.50	60 "
C. M. Callahan, Inc.....	4,245.00	60 "
James J. Conway.....	4,165.00	30 "
Louis Balboni.....	4,162.75	40 "
Carminc Russo.....	3,895.50	30 "
A. G. Tomasello & Son, Inc.*.....	3,267.50	30 "
L. P. Federico & Company †.....	2,262.50	50 "

* Awarded. Contract 879.

† Bid irregular.

APPENDIX XXXVIII.

BIDS FOR PLASTERING SHAWMUT STATION, DORCHESTER RAPID TRANSIT,
NOVEMBER 16, 1927.

BIDDER.	Estimated Price.	Time.
Austin M. Shea.....	\$8,020.00	30 days
A. V. Day & Company.....	6,135.00	40 "
Stephen F. Keith Company.....	5,971.50	40 "
George A. Craffey Company *.....	5,436.00	30 "

* Awarded. Contract 880.

APPENDIX XXXIX.

BIDS FOR BUILDING A SUB-POWER STATION ON BEALE STREET, DOR-
CHESTER, NOVEMBER 16, 1927.

BIDDER.	Estimated Price.	Time.
J. P. Foley & Sons, Inc.....	\$34,770.00	90 days
C. & R. Construction Company.....	21,000.00	150 "
J. F. Fitzgerald Construction Company...	20,948.00	70 "
John B. Dolan.....	20,430.00	80 "
Archdeacon & Sullivan.....	19,574.00	90 "
Samuel Pasquale.....	19,300.00	120 "
J. A. Singarella.....	18,940.00	55 "
A. G. Tomasello & Son, Inc.....	18,900.00	100 "
J. Slotnik Company.....	18,870.00	110 "
Hilton W. Long.....	18,858.00	70 "
A. Piotti Company.....	18,300.00	75 "
M. Solimando.....	16,875.00	85 "
John Bowen Company.....	16,840.00	90 "
M. S. Kelliher Company.....	16,748.00	88 "
John M. Tobin, Inc.....	16,700.00	70 "
Banspar Construction Company.....	16,197.00	100 "
White Construction Company *.....	16,033.00	110 "

* Awarded. Contract 881.

APPENDIX XL.

BIDS FOR INSTALLING VITRIFIED CLAY DUCTS AND A CONCRETE FOOT-WALK, ETC., NOVEMBER 29, 1927.

BIDDER.	Estimated Price.	Time.
Guiney & Hanson Construction Company.	\$9,485.50	60 days
M. Solimando.....	7,216.50	30 "
A. G. Tomasello & Son, Inc.....	6,310.00	30 "
J. A. Singarella.....	4,414.00	60 "
Hugh Nawn, Inc.....	4,329.50	30 "
C. M. Callahan, Inc.*.....	4,300.00	30 "

* Awarded. Contract 882.

APPENDIX XLI.

BIDS FOR BUILDING AN INCLOSED AREA, A STEAM HEATING PLANT AND A STEAM HEATING SYSTEM, NOVEMBER 29, 1927.

BIDDER.	Estimated Price.
George A. Fuller Company.....	\$214,785.00
A. G. Tomasello & Son, Inc.....	209,500.00
Walsh Brothers.....	201,244.00
Coleman Brothers, Inc.....	198,900.00
Banspar Construction Company.....	190,000.00
M. S. Kelliher Company.....	188,900.00
J. Slotnik Company.....	187,490.00
John Bowen Company.....	183,000.00
A. Piotti Company.....	178,800.00
J. A. Singarella.....	170,700.00
White Construction Company *.....	164,744.00

* Awarded. Contract 883.

APPENDIX XLII.

BIDS FOR INSTALLING A SYSTEM OF PLUMBING AT SHAWMUT STATION,
DORCHESTER RAPID TRANSIT, DECEMBER 8, 1927.

BIDDER.	Estimated Price.	Time.
William H. Mitchell & Son Company.....	\$2,925.00	25 days
Edward C. Kelly.....	2,800.00	42 "
Downey Company *.....	2,800.00	33 "

* Awarded. Contract 885.

APPENDIX XLII.

BIDS FOR FURNISHING AND DELIVERING, F. O. B. CARS, 14 CYPHER
STREET, SOUTH BOSTON, ABOUT 290 TONS OF STRUCTURAL STEEL,
CONSISTING OF BEAMS, CHANNELS, ANGLES AND ROLLED STEEL
SLABS, DECEMBER 8, 1927.

BIDDER.	Estimated Price.	Delivery.
Carnegie Steel Company.....	\$13,225.07	35 days
Bethlehem Steel Company *.....	13,046.82	45 "

* Awarded. Contract 886.

APPENDIX XLIII.

BIDS FOR FURNISHING GRANITE READY TO BE SET, FOR THE ENTRANCE
AND EXIT BUILDING AT ASHMONT STATION, DECEMBER 22, 1927.

BIDDER.	Estimated Price.	Time.
Estate of John Harrington.....	\$5,387.00	90 days
M. Solimando.....	4,795.00	70 "
Austin Ford & Son *.....	4,396.00	30 "

* Awarded. Contract 887.

APPENDIX XLIV.

BIDS FOR FURNISHING, READY TO ERECT, EIGHT ORNAMENTAL IRON SIGN POSTS AND FRAMES, WITH SIXTEEN BRONZE SASHES AND GLASS PAINTED SIGNS, DECEMBER 22, 1927.

BIDDER.	Estimated Price.	Time.
Flexlume Sign Company of New England..	\$11,828.00	60 days
Smith & Lovett Company.....	9,500.00	130 "
Barbour-Stockwell Company.....	8,937.00	77 "
Albany Malleable Iron Company.....	6,936.00	140 "
E. T. Ryan Iron Works, Inc.....	6,177.00	60 to 70 days
General Bank Equipment Company.....	6,015.00	90 days
The Lundin Electric & Machine Company *	4,960.00	60 "

* Awarded. Contract 888.

